



**HORIZONTAL CONTROL POINTS**  
(NAD83) 1986 ADJUSTMENT  
OHIO STATE PLANE COORDINATE SYSTEM - SOUTH ZONE

SOURCE	SHEET #	DESCRIPTION	NORTHING	EASTING
CP1	18	5/8" REBAR WITH RED PLASTIC CAP STAMPED "POMEROY TRAV PNT"	628099.98	1840658.93
CP2	16	5/8" REBAR WITH RED PLASTIC CAP STAMPED "POMEROY TRAV PNT"	628029.58	1841156.75
CP3	21	5/8" REBAR WITH RED PLASTIC CAP STAMPED "POMEROY TRAV PNT"	628047.54	1841633.87
CP4	17	5/8" REBAR WITH RED PLASTIC CAP STAMPED "POMEROY TRAV PNT"	627712.78	1841075.61
CP5	19	5/8" REBAR WITH RED PLASTIC CAP STAMPED "POMEROY TRAV PNT"	928447.85	1841131.36

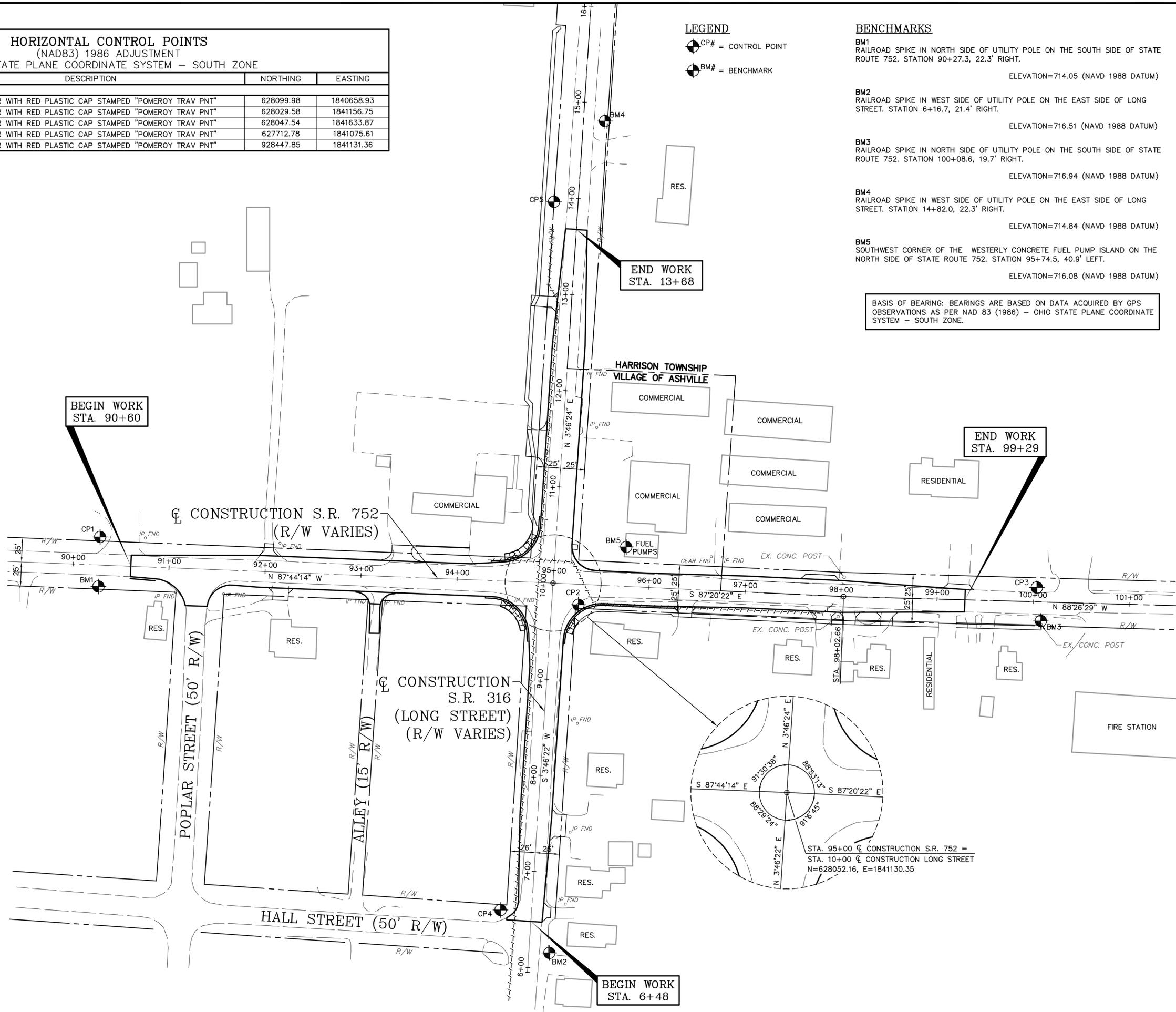
**LEGEND**

- CP# = CONTROL POINT
- BM# = BENCHMARK

**BENCHMARKS**

- BM1**  
RAILROAD SPIKE IN NORTH SIDE OF UTILITY POLE ON THE SOUTH SIDE OF STATE ROUTE 752. STATION 90+27.3, 22.3' RIGHT.  
ELEVATION=714.05 (NAVD 1988 DATUM)
- BM2**  
RAILROAD SPIKE IN WEST SIDE OF UTILITY POLE ON THE EAST SIDE OF LONG STREET. STATION 6+16.7, 21.4' RIGHT.  
ELEVATION=716.51 (NAVD 1988 DATUM)
- BM3**  
RAILROAD SPIKE IN NORTH SIDE OF UTILITY POLE ON THE SOUTH SIDE OF STATE ROUTE 752. STATION 100+08.6, 19.7' RIGHT.  
ELEVATION=716.94 (NAVD 1988 DATUM)
- BM4**  
RAILROAD SPIKE IN WEST SIDE OF UTILITY POLE ON THE EAST SIDE OF LONG STREET. STATION 14+82.0, 22.3' RIGHT.  
ELEVATION=714.84 (NAVD 1988 DATUM)
- BM5**  
SOUTHWEST CORNER OF THE WESTERLY CONCRETE FUEL PUMP ISLAND ON THE NORTH SIDE OF STATE ROUTE 752. STATION 95+74.5, 40.9' LEFT.  
ELEVATION=716.08 (NAVD 1988 DATUM)

BASIS OF BEARING: BEARINGS ARE BASED ON DATA ACQUIRED BY GPS OBSERVATIONS AS PER NAD 83 (1986) - OHIO STATE PLANE COORDINATE SYSTEM - SOUTH ZONE.

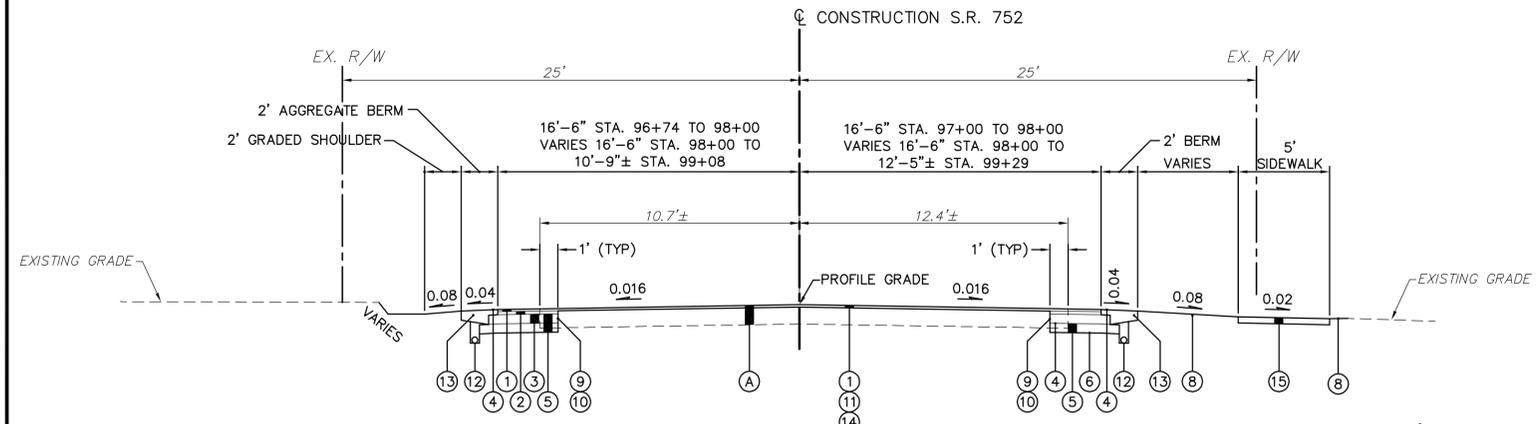


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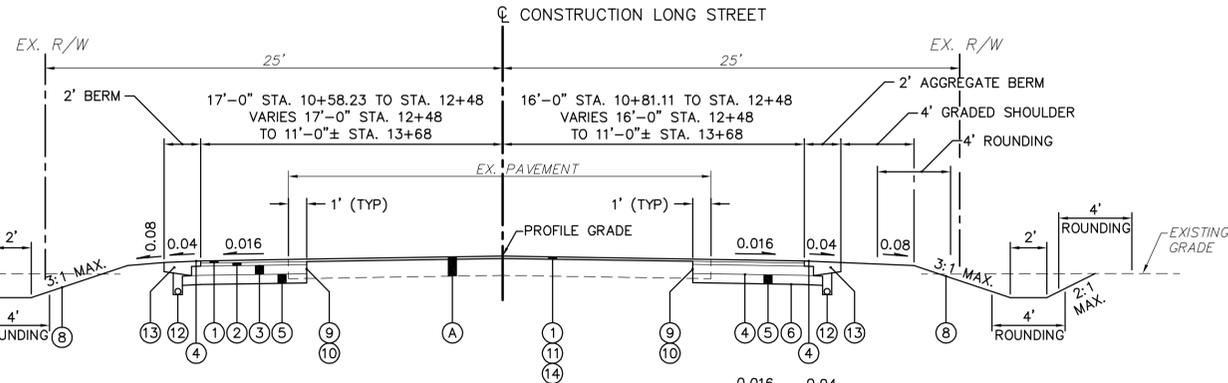
**SCHEMATIC PLAN**  
**STATE ROUTE 752 AND LONG STREET INTERSECTION**

**PIC-752/LONG ST.**

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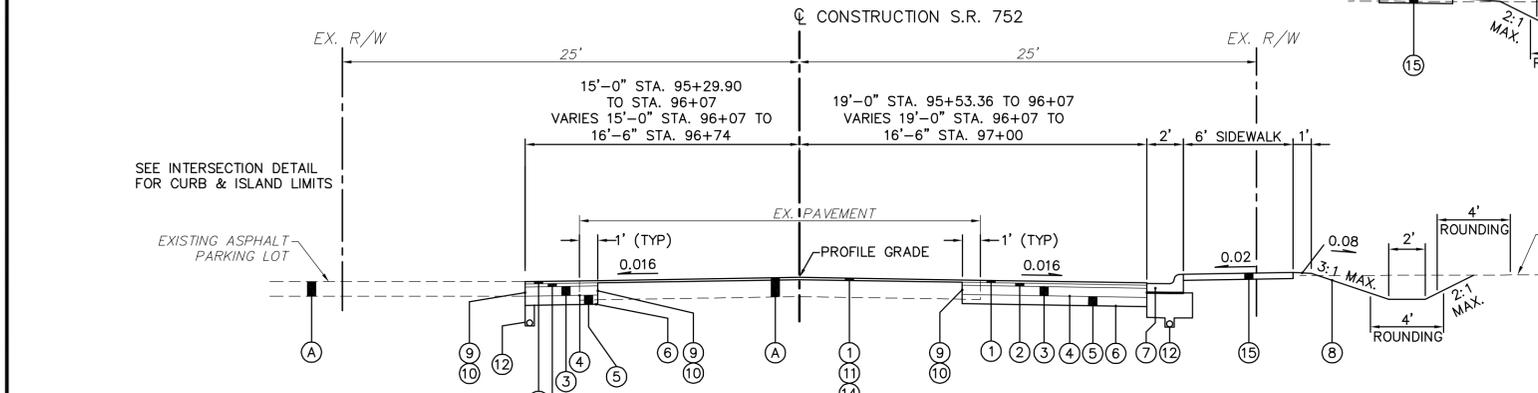


**S.R. 752**  
 STA. 96+74 TO 99+29 LEFT  
 STA. 96+28 TO 99+29 RIGHT  
 SCALE: 1" = 5'

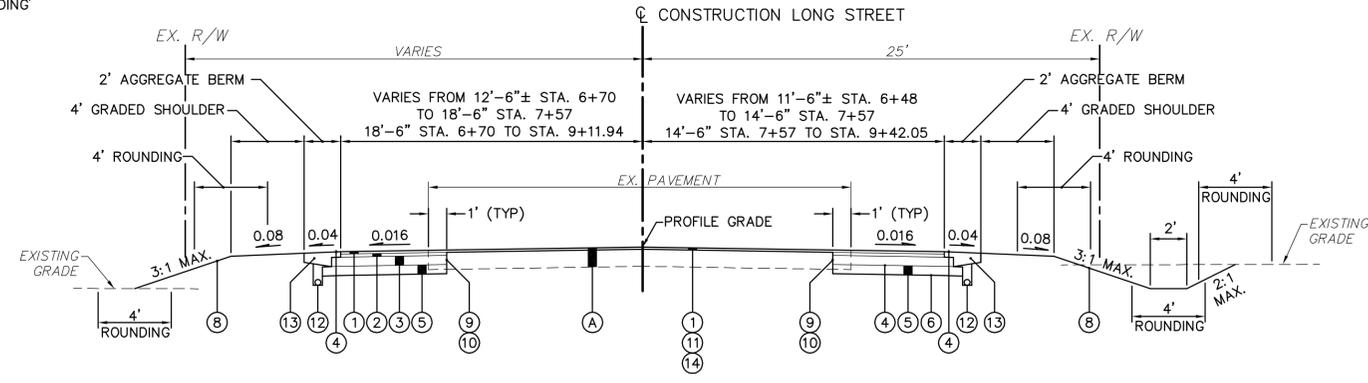


**LONG STREET**  
 STA. 10+55.23 TO 13+68 LEFT  
 STA. 10+81.11 TO 13+68 RIGHT  
 SCALE: 1" = 5'

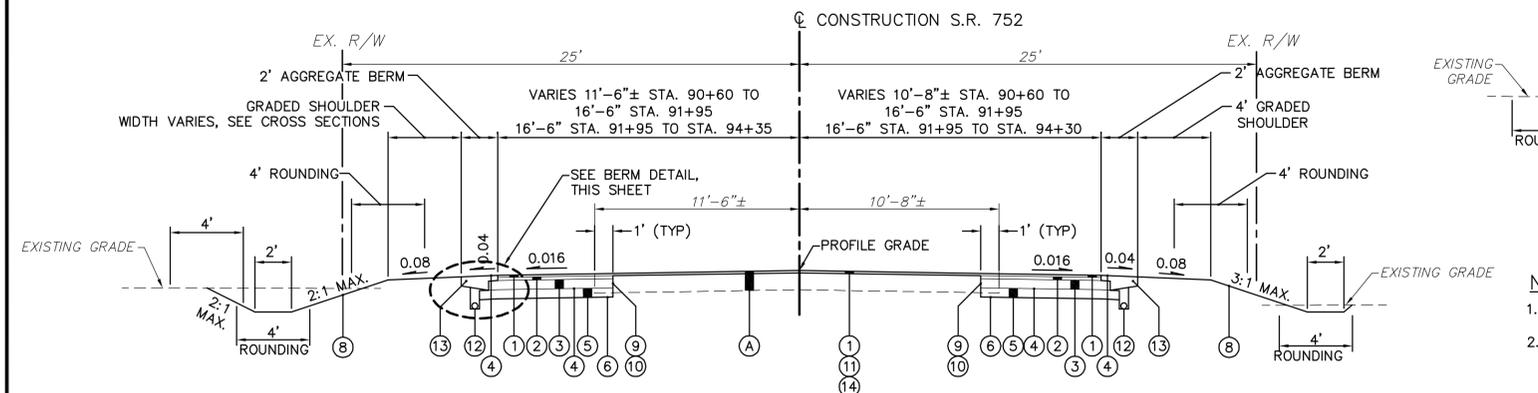
**LONG STREET SHOULDER DETAIL**  
 STA. 10+55.23 TO 11+63.21 RIGHT  
 SCALE: 1" = 5'



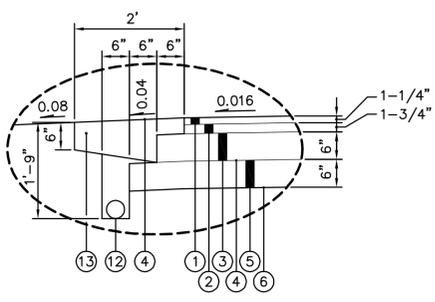
**S.R. 752**  
 STA. 95+29.90 TO 96+74 LEFT  
 STA. 95+53.36 TO 96+28 RIGHT  
 SCALE: 1" = 5'



**LONG STREET**  
 STA. 6+48 TO 9+11.94 LEFT  
 STA. 6+48 TO 9+42.05 RIGHT  
 SCALE: 1" = 5'



**S.R. 752**  
 STA. 90+60 TO 94+35 LEFT  
 STA. 90+60 TO 94+30 RIGHT  
 ON RIGHT SIDE WEST OF POPLAR STREET, PROVIDE RESURFACING ONLY AND MATCH EXISTING PAVEMENT WIDTH.  
 SCALE: 1" = 5'



**NOTES:**

- PAVEMENT PLANING SHALL BE A CONSTANT DEPTH OF 1 1/4" EXCEPT WHERE NOTED OTHERWISE.
- THE EXISTING PAVEMENT EDGES SHALL BE SAWCUT TO LOCATE A SOUND PAVEMENT EDGE. THE LOCATION OF THE SAWCUT SHALL ALLOW FOR A MINIMUM PROPOSED PAVEMENT WIDTH OF 2 FEET. FOR ESTIMATING PURPOSES, PAVEMENT CALCULATIONS INCLUDED IN THE PLAN INDICATE AN AVERAGE WIDTH OF 1 FOOT.
- SEE SHEET 33/37 FOR INTERSECTION AND CURB DETAILS.
- SEE CROSS SECTION SHEETS FOR DITCH LOCATIONS.

**ITEM LEGEND**

- |  |   |
|--|---|
| ① ITEM 448 - 1-1/4" ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG 64-22 (O.D.O.T.)                             | ⑨ ITEM 423 - CRACK SEALING, TYPE II (O.D.O.T.)  |
| ② ITEM 448 - 1-3/4" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, PG 64-22 (O.D.O.T.)                        | ⑩ ITEM 252 - FULL DEPTH SAWCUT                  |
| ③ ITEM 301 - 6" BITUMINOUS AGGREGATE BASE, PG 64-22  | ⑪ ITEM 407 - TACK COAT (0.10 GAL./S.Y.)         |
| ④ ITEM 408 - BITUMINOUS PRIME COAT, 702.02, RC-250, MC-30, 702.03, MC-250 (APPLIED AT 0.40 GAL. PER SQ. YD.) | ⑫ ITEM 605 - 4" PIPE UNDERDRAIN                 |
| ⑤ ITEM 304 - 6" AGGREGATE BASE   | ⑬ ITEM 304 - 9" AGGREGATE BERM                  |
| ⑥ ITEM 203 - SUBGRADE COMPACTION   | ⑭ ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE |
| ⑦ ITEM 609 - STANDARD CONCRETE COMBINED CURB & GUTTER (COC STD DWG 2010)                                     | ⑮ ITEM 608 - CONCRETE WALK                      |
| ⑧ ITEM 659 - SEEDING AND MULCHING  | Ⓐ EXISTING PAVEMENT                             |

**GENERAL NOTES**

**GEN 1** THE VILLAGE OF ASHVILLE REQUIREMENTS, TOGETHER WITH THE LATEST EDITION OF THE STATE OF OHIO DEPARTMENT OF TRANSPORTATION AND THE CITY OF COLUMBUS CONSTRUCTION AND MATERIAL SPECIFICATION, SHALL GOVERN ALL CONSTRUCTION ITEMS, MATERIAL, WORKSMANSHIP, ETC. THAT ARE A PART OF THIS PLAN, IN FORCE ON THE DATE OF CONTRACT, UNLESS OTHERWISE NOTED. EXCEPT AS SUCH SPECIFICATIONS ARE MODIFIED BY THE FOLLOWING GENERAL NOTES AND/OR SPECIFICATIONS OR BY THE CONSTRUCTION DETAILS SET FORTH HEREIN. THE CONTRACTOR SHALL ALSO CONFORM TO REQUIREMENTS OF THE VILLAGE OF ASHVILLE GENERAL PROVISIONS AND THE STANDARD CITY OF COLUMBUS DETAIL CONSTRUCTION DRAWINGS. IF THERE ARE ANY DISCREPANCIES, THE VILLAGE OF ASHVILLE REQUIREMENTS SHALL GOVERN.

**GEN 2** ANY MODIFICATION TO THE SPECIFICATIONS OR CHANGES TO WORK AS SHOWN ON THESE DRAWINGS MUST HAVE PRIOR WRITTEN APPROVAL BY THE VILLAGE ENGINEER.

**GEN 3** THE CONTRACTOR OR SUBCONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR COMPLYING WITH ALL FEDERAL, STATE, AND LOCAL SAFETY REQUIREMENTS, TOGETHER WITH EXERCISING PRECAUTIONS AT ALL TIMES FOR PROTECTION OF PERSONS (INCLUDING EMPLOYEES) AND PROPERTY. IT IS ALSO THE SOLE RESPONSIBILITY OF THE CONTRACTOR OR SUBCONTRACTOR TO INITIATE, MAINTAIN, AND SUPERVISE ALL SAFETY REQUIREMENTS, PRECAUTIONS, AND PROGRAMS IN CONNECTION WITH THE WORK. THE CONTRACTOR OR SUBCONTRACTOR SHALL ALSO ABIDE BY ALL ORDINANCES OF THE VILLAGE OF ASHVILLE, OHIO.

**GEN 4** PRIOR TO BIDDING, THE CONTRACTOR SHALL, BY PERSONAL EXAMINATION, SATISFY THEMSELVES AS TO THE LOCATION OF THE PROPOSED WORK AND TO ACQUAINT THEMSELVES THOROUGHLY WITH THE EXISTING CONDITIONS AND THE DIFFICULTIES THAT ARE LIKELY TO BE ENCOUNTERED IN THE PERFORMANCE OF THE PROPOSED WORK. THIS IS ESPECIALLY TRUE WITH REGARD TO ANY REMOVAL ITEMS.

**GEN 5** ALL WORK SHALL BE COMPLETELY ACCEPTABLE TO THE VILLAGE OF ASHVILLE OFFICIALS. NO WORK SHALL BE COMMENCED UNTIL ARRANGEMENTS HAVE BEEN MADE WITH THE VILLAGE OF ASHVILLE ENGINEER FOR INSPECTION. NECESSARY LINE AND GRADE STAKING WILL BE PROVIDED BY THE CONTRACTOR. CUT SHEETS SHALL BE SUBMITTED AND APPROVED BY THE VILLAGE PRIOR TO THE BEGINNING OF CONSTRUCTION.

**GEN 6** PRIOR TO BEGINNING CONSTRUCTION, THE CONTRACTOR SHALL MAKE ALL ARRANGEMENTS NECESSARY TO COORDINATE AND PROVIDE FULL-TIME INSPECTION SERVICE BY THE VILLAGE FOR THE PROPOSED WORK.

**GEN 7** THE CONTRACTOR SHALL PROVIDE WRITTEN NOTIFICATION TO THE VILLAGE OF ASHVILLE ENGINEERING DEPARTMENT AT LEAST SEVEN (7) DAYS PRIOR TO ANY CONSTRUCTION.

**GEN 9** THE CONTRACTOR SHALL NOTIFY ALL OF THE AFFECTED PROPERTY OWNERS AT LEAST TWO (2) WEEKS PRIOR TO THE SCHEDULED COMMENCEMENT OF WORK ON THE JOB SITE. THIS LETTER SHALL STATE THE DATE CONSTRUCTION IS TO BEGIN AND THAT RESIDENTS SHOULD REMOVE ANY ITEMS, EXAMPLE: SHRUBS, SPOT LIGHTS, FENCES, STONES OR BRICKS, ETC., WHICH MAY FALL WITHIN THE WORK LIMITS. THE LETTER SHOULD ALSO CONTAIN THE NAME AND A TWENTY-FOUR (24) HOUR PHONE NUMBER OF A CONTACT, FROM THE CONTRACTOR'S FIRM, WHO COULD BE CONTACTED IN CASE OF EMERGENCY. THIS LETTER, FOR CONVENIENCE PURPOSES, SHALL CONTAIN A PHONE NUMBER FOR ALL LOCAL LAW ENFORCEMENT AGENCIES AND FIRE SAFETY FORCES.

**GEN 10** EXISTING UTILITIES SHOWN ARE FROM BEST AVAILABLE RECORDS AND FIELD INVESTIGATION, AND ARE NOT NECESSARILY COMPLETE OR EXACT. THE CONTRACTOR IS RESPONSIBLE FOR THE INVESTIGATION, LOCATION, SUPPORT, PROTECTION AND RESTORATION OF ALL EXISTING UTILITIES AND APPURTENANCES WHETHER SHOWN ON THESE PLANS OR NOT. THE CONTRACTOR SHALL EXPOSE ALL UTILITIES OR

**GENERAL NOTES (CONTINUED)**

**GEN 10 (CONTINUED)** STRUCTURES PRIOR TO CONSTRUCTION TO VERIFY THE VERTICAL AND HORIZONTAL EFFECT OF THE PROPOSED CONSTRUCTION, AND SHALL MAKE ADJUSTMENTS IN ELEVATIONS AS DIRECTED BY THE VILLAGE ENGINEER TO PROVIDE SUFFICIENT CLEARANCE BETWEEN THE PROPOSED AND EXISTING UTILITIES. THE CONTRACTOR SHALL CONTACT THE OHIO UTILITIES PROTECTION SERVICE (OUPS) AT 1-800-362-2764 THREE (3) WORKING DAYS PRIOR TO WORK IN THE VICINITY OF THEIR UNDERGROUND LINES AND ACCORDING TO 153.64, OHIO REVISED CODE (ORC) SHALL NOTIFY ALL UTILITY COMPANIES AT LEAST FORTY-EIGHT (48) HOURS PRIOR TO WORK IN THE VICINITY OF THEIR UNDERGROUND LINES.

**GEN 11** THE FOLLOWING UTILITY OWNERS ARE LOCATED WITHIN THE WORK LIMITS OF THIS PROJECT AND SUBSCRIBE TO A REGISTERED UNDERGROUND UTILITY SERVICE, UNLESS NOTED OTHERWISE:

VERIZON  
500 LANCASTER PIKE  
CIRCLEVILLE, OHIO 43113  
(740) 474-5033

AMERICAN ELECTRIC POWER  
1320 SUGAR GROVE ROAD  
LANCASTER, OHIO 43130  
(740) 689-4700

SOUTH CENTRAL POWER CO.  
2780 COONPATH ROAD, NE  
LANCASTER, OHIO 43130  
(614) 653-4422

SBC  
140 WEST WHEELING STREET  
LANCASTER, OHIO 43130  
(740) 687-6696

TIME WARNER COMMUNICATIONS  
1315 GRANVILLE PIKE  
LANCASTER, OHIO 43130  
(740) 635-9685

\*VILLAGE OF ASHVILLE  
SERVICE DEPT.  
160 CHERRY STREET  
ASHVILLE, OHIO 43103  
(740) 983-4053

COLUMBIA GAS OF OHIO  
843 PLATT AVENUE  
CHILLICOTHE, OHIO 45601  
(614) 772-9224

WATERLINE SANITARY SEWER  
& STORM SEWER ONLY  
200 EAST STATION STREET  
ASHVILLE OHIO 43103  
(740) 983-6367

\* THE VILLAGE OF ASHVILLE DOES NOT SUBSCRIBE NOR IS A MEMBER OF OUPS.

**GEN 12** THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES AND WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE VILLAGE OF ASHVILLE AND/OR ENGINEER ASSUMES NO RESPONSIBILITY AS TO THE ACCURACY OR DEPTHS OF THE UNDERGROUND FACILITIES AS SHOWN ON THE PLANS. THE CONTRACTOR MUST CALL APPROPRIATE UTILITY COMPANY AT LEAST SEVEN (7) DAYS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATION OF UTILITIES. IT SHALL BE THE RESPONSIBILITY FOR THE CONTRACTOR TO RELOCATE ALL EXISTING UTILITIES WHICH CONFLICT WITH THE PROPOSED IMPROVEMENTS SHOWN ON THE PLANS.

**GEN 13** THE CONTRACTOR IS RESPONSIBLE FOR COORDINATING THE RELOCATION OF ANY UTILITIES AS REQUIRED BY THE PLAN WITH THE OWNER OF THE AFFECTED UTILITY.

**GEN 14** WHERE POTENTIAL GRADE CONFLICTS MIGHT OCCUR WITH EXISTING UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO UNCOVER SUCH UTILITIES SUFFICIENTLY IN ADVANCE OF LAYING PIPE OR DUCT IN ORDER THAT THE ENGINEER MAY DETERMINE THE EXACT ELEVATION AND MAKE ANY NECESSARY ADJUSTMENTS. COST OF THE ABOVE SHALL BE INCLUDED IN THE PRICE BID FOR THE VARIOUS ITEMS IN THE CONTRACT.

**GEN 15** ALL MATERIALS INCLUDING PIPING, APPURTENANCES, MANHOLES, GRAVEL, ETC. TO BE UTILIZED FOR DEDICATED PUBLIC UTILITIES OR ROADWAYS MUST BE APPROVED BY THE VILLAGE OF ASHVILLE ENGINEER.

**GENERAL NOTES (CONTINUED)**

**GEN 16** ALL FIELD TILES BROKEN DURING EXCAVATION SHALL BE REPLACED BY THE CONTRACTOR TO ORIGINAL CONDITION OR CONNECTED TO THE PROPOSED UNDERDRAIN OR TO THE STORM SEWER SYSTEMS AS DIRECTED BY THE ENGINEER.

**GEN 17** TWENTY-FOUR (24) HOUR ADVANCE NOTIFICATION IS REQUIRED FOR ALL WORK REQUIRING INSPECTION, TESTING, OR APPROVAL BY THE VILLAGE ENGINEER OR BUILDING DEPARTMENTS.

**GEN 18** THE CONTRACTOR SHALL REPAIR OR REPLACE ANY AND ALL EXISTING WORK DAMAGED DURING OR DUE TO THE EXECUTION OF THIS CONTRACT TO EQUAL OR BETTER CONDITION PRIOR TO THE DAMAGE, AT THEIR OWN EXPENSE. ALL SAID WORK SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE VILLAGE OF ASHVILLE'S ENGINEER. ANY DAMAGE TO OTHER UTILITIES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED BY THE APPROPRIATE UTILITY COMPANY AT THE CONTRACTOR'S EXPENSE.

**GEN 19** CARE SHALL BE EXERCISED WHEN WORKING AROUND EXISTING TREES AND SHRUBS. ANY TREES OR SHRUBS NOT MARKED FOR REMOVAL THAT ARE DAMAGED BY THE CONTRACTOR WILL HAVE TO BE REPLACED BY THE CONTRACTOR TO THE SATISFACTION OF THE OWNER.

**GEN 21** EXISTING STRUCTURES TO BE REMOVED OR DEMOLISHED REQUIRE A "DEMOLITION PERMIT" ISSUED BY THE VILLAGE OF ASHVILLE ENGINEER.

**GEN 22** THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING MAILBOXES AND ACCESS FOR MAIL SERVICE IN THE CONSTRUCTION AREA.

**GEN 23** THE CONTRACTOR SHALL NOT ORDER MATERIAL OR PERFORM WORK FOR ITEMS DESIGNATED BY PLAN NOTE TO BE USED "AS DIRECTED BY THE ENGINEER" UNLESS AUTHORIZED BY THE ENGINEER. THE ACTUAL WORK LOCATIONS AND QUANTITIES USED FOR SUCH ITEMS SHALL BE INCORPORATED INTO THE FINAL CHANGE ORDER GOVERNING COMPLETION OF THIS PROJECT.

**GEN 24** ALL ITEMS OF WORK CALLED FOR ON THE PLANS FOR WHICH NO SPECIFIC METHOD OF PAYMENT IS PROVIDED SHALL BE PERFORMED BY THE CONTRACTOR AND THE COST OF IT SHALL BE INCLUDED IN THE PRICE BID FOR THE VARIOUS RELATED ITEMS.

**GEN 25** ALL EXCAVATION ON THIS PROJECT IS UNCLASSIFIED. THE CONTRACTOR SHALL MAKE ALL EXCAVATION OF WHATEVER NATURE NECESSARY FOR CONSTRUCTION OF WATER LINES AND SEWERS AND THEIR APPURTENANT STRUCTURES INCLUDED IN THIS PROJECT.

**GEN 28** THE CONTRACTOR IS RESPONSIBLE TO NOTIFY THE VILLAGE ENGINEER AND REQUEST A FINAL PUNCH-OUT INSPECTION OF THE SITE ONCE ALL OF THE ITEMS ON THE APPROVED DEVELOPMENT PLANS HAVE BEEN COMPLETED.

**GEN 29** THE CONTRACTOR SHALL SUFFICIENTLY EXPOSE EACH UTILITY OR STRUCTURE INDICATED ON THE PLANS IN ADVANCE OF EXCAVATING TO DETERMINE IF A GRADE CONFLICT OCCURS. LOCATIONS SHOWN ARE APPROXIMATE. OTHER UTILITIES NOT MARKED MAY ALSO REQUIRE EXPOSING.

**GEN 31** ACCESS TO ALL ADJOINING PROPERTIES SHALL BE MAINTAINED AT ALL TIMES, UNLESS OTHERWISE SHOWN ON THE DRAWINGS.

**GEN 33** ALL STUMPS AND TREES, EITHER STANDING OR FALLEN, MARKED FOR REMOVAL AND LOCATED WITHIN THE CONSTRUCTION LIMITS SHALL BE REMOVED UNDER THE LUMP SUM BID FOR ITEM 201, CLEARING AND GRUBBING, UNLESS DIRECTED OTHERWISE.

**GENERAL NOTES (CONTINUED)**

**GEN 34** AT ALL UTILITY CROSSINGS, THE BACKFILL SHALL CONSIST OF COMPACTED GRANULAR MATERIAL IN ACCORDANCE WITH C.M.S.C. ITEM 912 BETWEEN THE DEEPER AND SHALLOWER PIPE. WHERE PROPOSED UTILITIES OR SERVICES CROSS PROPOSED OR EXISTING PAVEMENT AREAS, BACKFILL SHALL BE COMPACTED GRANULAR MATERIAL IN ACCORDANCE WITH C.M.S.C. ITEM 912 EXTENDING AT LEAST THREE (3) FEET BEYOND THE BACK OF CURB OR EDGE OF PAVEMENT. THE COST IS TO BE INCLUDED IN THE PRICE BID FOR THE RELATED PIPE ITEMS.

**GEN 35** IN THE EVENT THAT IT BECOMES NECESSARY, THE VILLAGE OF ASHVILLE SHALL PERFORM WORK OF AN IMMEDIATE NATURE REQUIRED OF THE CONTRACTOR BY THIS CONTRACT BECAUSE OF THE FAILURE OR REFUSAL OF THE CONTRACTOR TO PERFORM-SUCH WORK, THE CONTRACTOR SHALL REIMBURSE THE VILLAGE OF ASHVILLE AT THE RATE OF 2.5 TIMES THE ACTUAL COST OF LABOR, MATERIALS AND EQUIPMENT NECESSARY TO PERFORM SUCH WORK. THE VILLAGE OF ASHVILLE SHALL BE REQUIRED TO NOTIFY OR ATTEMPT TO NOTIFY THE DESIGNATED REPRESENTATIVE OF THE CONTRACTOR OF THE NECESSITY TO PERFORM SUCH WORK. IN THE EVENT OF AN EMERGENCY, NO NOTIFICATION IS REQUIRED. IF THE CONTRACTOR REFUSES OR FAILS WITHIN A REASONABLE TIME TO PERFORM OR CAUSE THE PERFORMANCE OF SUCH WORK, THE VILLAGE OF ASHVILLE SHALL BE REIMBURSED BY THE CONTRACTOR IN THE AMOUNT PROVIDED HEREIN. REASONABLE TIME FOR ALL STREETS INVOLVED ON THIS CONTRACT IS ONE (1) HOUR FROM THE TIME OF NOTIFICATION BY THE VILLAGE OF ASHVILLE.

**GEN 37** ALL SIDEWALKS, CURB RAMPS, AND CURBS AND GUTTERS SHALL MEET FEDERAL A.D.A. (AMERICANS WITH DISABILITIES ACT) REQUIREMENTS, LATEST EDITION.

**GEN 38** ANY EXISTING PAVEMENT REMOVED FOR SANITARY SEWER, STORM SEWER OR WATERLINE PLACEMENT SHALL BE BACKFILLED AND COMPACTED TO A MAXIMUM DENSITY OF 100% OR WITH CONTROLLED DENSITY FILL (CDF), REFERENCE CITY OF COLUMBUS ITEM 912 OR 636. IF CDF IS USED ALL DUCTILE OR CAST IRON PIPE IN CONTACT WITH CDF SHALL BE POLY-WRAPPED IN ACCORDANCE WITH C-7484 OR AWWA C-105.

**GEN 39** ANY TUNNELING OR BORING OF ANY PIPELINES SHALL BE IN ACCORDANCE WITH PLANS APPROVED BY THE VILLAGE ENGINEER.

**GEN 40** DURING THE CONSTRUCTION OF THE IMPROVEMENTS SHOWN ON THESE PLANS, SEDIMENTATION AND EROSION CONTROL SHALL BE CONTROLLED IN ACCORDANCE WITH APPLICABLE STATE AND FEDERAL REGULATIONS. THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING EROSION AND SEDIMENT CONTROL IN ACCORDANCE WITH EPA STANDARDS WHERE NEEDED FOR THE DURATION OF THE PROJECT. ALL COSTS ASSOCIATED WITH SEDIMENTATION AND EROSION CONTROL SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE PIPE. SEE NOTES ON SHEET 7/37.

**GEN 41** THE CONTRACTOR SHALL, PRIOR TO BIDDING, MAKE SITE VISITS TO SATISFY HIMSELF OF THE SITE CONDITIONS. THE CONTRACTOR SHALL, AS A PART OF HIS BID, SUBMIT A DETAILED DEWATERING PLAN THAT SHALL INCLUDE THE NUMBER AND SPACING OF WELL POINTS, THE SIZE OF PUMPS TO BE USED, THE POINTS OF DISCHARGE OF SAID DEWATERING OPERATIONS, AND ANY OTHER INFORMATION PERTINENT TO THE DEWATERING OF THE PROJECT. NO EXTRA PAYMENT SHALL BE MADE FOR DEWATERING. ALL COSTS FOR DEWATERING SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ASSOCIATED ITEMS. BY SUBMITTING A BID THE CONTRACTOR IS STATING THAT HE CLEARLY UNDERSTANDS THE SCOPE OF THE DEWATERING FOR THE PROJECT AND HAS INCLUDED THIS WORK IN HIS UNIT PRICE BID FOR ASSOCIATED ITEMS.

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GENERAL NOTES  
LONG STREET AND STATE ROUTE 752

PIC-752/LONG ST.

**GENERAL NOTES (CONTINUED)**

**GEN 43** THE CONTRACTOR SHALL CLEAN UP ALL DEBRIS AND MATERIALS RESULTING FROM HIS OPERATIONS AND RESTORE ALL SURFACES, STRUCTURES, DITCHES, AND PROPERTY TO ITS ORIGINAL CONDITION TO THE SATISFACTION OF THE ENGINEER.

**GEN 44** THE CONTRACTOR SHALL CONFINE HIS ACTIVITIES TO THE PROJECT SITE UNDER DEVELOPMENT OR THE EXISTING RIGHT-OF-WAYS, CONSTRUCTION, AND PERMANENT EASEMENTS AND SHALL NOT TRESPASS UPON OTHER PRIVATE PROPERTY WITHOUT THE WRITTEN CONSENT OF THE OWNER.

**GEN 45** THE CONTRACTOR SHALL DISPOSE OF ALL SURPLUS EXCAVATION OFF-SITE. IF SURPLUS EXCAVATION IS MOUNDED WITHIN VILLAGE CORPORATION LIMITS, THE MOUND SHALL BE STABILIZED AND SEEDED ACCORDING TO OEPA SPECIFICATIONS. SILT FENCE IS REQUIRED AROUND THE ENTIRE BASE OF THE MOUND. ANY MOUNDING SHALL NOT INTERFERE WITH DRAINAGE. THE VILLAGE RESERVES THE RIGHT TO LIMIT THE HEIGHT OF ANY MOUNDING.

**GEN 46** THE INFORMATION SHOWN CONCERNING EXISTING UTILITIES IS NOT REPRESENTED, WARRANTED, OR GUARANTEED TO BE COMPLETE OR ACCURATE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PHYSICALLY LOCATE AND VERIFY, IN THE FIELD, ALL UTILITY LOCATIONS AND ELEVATIONS, WHETHER SHOWN ON THE PLAN OR NOT, PRIOR TO THE BEGINNING OF CONSTRUCTION OPERATIONS. THE CONTRACTOR SHALL SUPPORT, PROTECT, AND RESTORE ALL EXISTING UTILITIES AND THEIR ASSOCIATED ITEMS.

**GEN 47** COMPLIANCE WITH THE OCCUPATIONAL SAFETY AND HEALTH ACT OF 1970 IS REQUIRED OF ALL CONTRACTORS ON THIS PROJECT.

**GEN 48** WHERE MANHOLES AND OTHER STORM SEWER STRUCTURES ARE LOCATED WITHIN STREET GRADING LIMITS, THE TOPS SHALL BE BUILT TO ELEVATIONS SHOWN ON THE PLANS. ELSEWHERE THEY SHALL BE BUILT OR SUBSEQUENTLY ADJUSTED TO MEET SURFACE GRADES ESTABLISHED FOR THE PROJECT. THE COST OF THE ADJUSTMENT IS TO BE INCLUDED IN THE PRICE BID FOR THE STRUCTURE.

**GEN 49** THE CONTRACTOR SHALL CLEAN UP ALL DEBRIS AND MATERIAL RESULTING FROM HIS OPERATION AND RESTORE ALL OTHER SURFACES, STRUCTURES, DITCHES, ON PUBLIC AND ADJACENT PROPERTY TO ORIGINAL CONDITIONS TO THE SATISFACTION OF THE ENGINEER AND THE RESPECTIVE PROPERTY OWNERS. ALL COSTS FOR THIS WORK SHALL BE INCLUDED WITH THE PRICE BID FOR THE VARIOUS ITEMS.

**SUPPLEMENTAL NOTES**

**ROUNDING**

THE ROUNDING AT SLOPE BREAKPOINTS SHOWN ON THE TYPICAL SECTIONS APPLY TO ALL CROSS SECTIONS EVEN THOUGH OTHERWISE SHOWN.

**FIELD SURVEY**

THESE PLANS WERE PREPARED BASED ON A FIELD SURVEY PREPARED BY SHAFFER POMEROY, LTD.

**COORDINATES**

NORTHING AND EASTING COORDINATES LISTED WITHIN THIS SET OF PLANS IS BASED ON OHIO STATE PLANE COORDINATES, SOUTH ZONE.

**EXPOSE**

THE CONTRACTOR SHALL EXPOSE THE UTILITY OR STRUCTURE INDICATED SUFFICIENTLY IN ADVANCE OF LAYING A PROPOSED STORM SEWER OR WATER LINE, AND SHALL EXERCISE EXTREME CAUTION NOT TO DAMAGE THE EXISTING UTILITY. THE COST TO EXPOSE THE UTILITY OR STRUCTURE SHALL BE INCLUDED UNDER ITEM 901 AND/OR ITEM 801. THE CONTRACTOR IS RESPONSIBLE FOR ANY DAMAGE INCURRED TO THE EXISTING UTILITY. IN CASE OF CONFLICT, THE VILLAGE OF ASHVILLE SHALL BE NOTIFIED FOR PLAN REVISION.

**SUPPLEMENTAL NOTES (CONTINUED)**

**SEEDING AND MULCHING**

THE FOLLOWING QUANTITIES ARE PROVIDED TO PROMOTE GROWTH AND CARE OF PERMANENT SEEDED AREAS:

- 659, TOPSOIL 309 CU. YD.
- 659, SEEDING AND MULCHING 2777 SQ. YD.
- 659, INTERSEEDING 139 SQ. YD.
- 659, COMMERCIAL FERTILIZER 0.39 TON
- 659, LIME 1.2 TON
- 659, WATER 15.75 M. GAL.
- 659, MOWING 6.3 M. SQ. FT.

SEEDING AND MULCHING SHALL BE APPLIED TO ALL AREAS OF EXPOSED SOIL BETWEEN THE RIGHT-OF-WAY LINES, AND WITHIN THE CONSTRUCTION LIMITS FOR AREAS OUTSIDE THE RIGHT-OF-WAY LINES COVERED BY WORK AGREEMENT OR SLOPE EASEMENT. QUANTITY CALCULATIONS FOR SEEDING AND MULCHING ARE BASED ON THESE LIMITS.

**PAVEMENT PLANING, AS PER PLAN**

THE SPOILS FROM THE PAVEMENT PLANING SHALL BE PLACE IN A STOCKPILE (OFF SITE) AS DIRECTED BY THE VILLAGE ENGINEER. THE SPOILS WILL BECOME THE PROPERTY OF THE VILLAGE OF ASHVILLE. LOCATION OF STOCKPILE SHALL BE WITHIN THE VILLAGE OF ASHVILLE'S COOPERATION LIMITS. ALL COSTS TO BE INCLUDED IN ITEM 254 - PAVEMENT PLANING, AS PER PLAN.

**CONNECTION TO EXISTING PIPE**

WHERE PLANS PROVIDE FOR A PROPOSED CONDUIT TO BE CONNECTED TO, OR CROSS OVER OR UNDER AN EXISTING SEWER OR UNDERGROUND UTILITY, THE CONTRACTOR SHALL LOCATE THE EXISTING PIPES OR UTILITIES BOTH AS TO LINE AND GRADE BEFORE STARTING TO LAY THE PROPOSED CONDUIT.

IF IT IS DETERMINED THAT THE ELEVATION OF THE EXISTING CONDUIT, OR EXISTING APPURTENANCE TO BE CONNECTED, DIFFERS FROM THE PLAN ELEVATION OR RESULTS IN A CHANGE IN THE PLAN CONDUIT SLOPE, THE ENGINEER SHALL BE NOTIFIED BEFORE STARTING CONSTRUCTION OF ANY PORTION OF THE PROPOSED CONDUIT WHICH WILL BE AFFECTED BY THE VARIANCE IN THE EXISTING ELEVATIONS.

IF IT IS DETERMINED THAT THE PROPOSED CONDUIT WILL INTERSECT AN EXISTING SEWER OR UNDERGROUND UTILITY IF CONSTRUCTED AS SHOWN ON THE PLAN, THE ENGINEER SHALL BE NOTIFIED BEFORE STARTING CONSTRUCTION OF ANY PORTION OF THE PROPOSED CONDUIT WHICH WOULD BE AFFECTED BY THE INTERFERENCE WITH AN EXISTING FACILITY.

PAYMENT FOR ALL THE OPERATIONS DESCRIBED ABOVE SHALL BE INCLUDED IN THE CONTRACT PRICE FOR THE PERTINENT 603 CONDUIT ITEM.

**BENCHMARKS AND PROPERTY CORNERS**

DO NOT DISTURB PERMANENT BENCHMARKS (MONUMENTS) AND PROPERTY CORNERS. IF DISTURBED DURING CONSTRUCTION, THE BENCHMARK OR PROPERTY CORNER SHALL BE REPLACED AND CERTIFIED BY AN OHIO REGISTERED PROFESSIONAL SURVEYOR AT THE CONTRACTOR'S EXPENSE.

**USE OF FIRE HYDRANTS**

THE CONTRACTOR SHALL MAKE THE PROPER ARRANGEMENTS WITH THE VILLAGE OF ASHVILLE UTILITY DEPARTMENT FOR THE USE OF FIRE HYDRANTS WHEN USED FOR WORK PERFORMED UNDER THIS CONTRACT.

**REVIEW OF DRAINAGE FACILITIES**

BEFORE ANY WORK IS STARTED ON THE PROJECT, AND AGAIN BEFORE FINAL ACCEPTANCE BY THE VILLAGE OF ASHVILLE, THE CONTRACTOR, ALONG WITH THE LOCAL REPRESENTATIVES, SHALL MAKE AN INSPECTION OF THE EXISTING SEWERS WITHIN THE WORK LIMITS WHICH ARE TO REMAIN IN SERVICE AND WHICH MAY BE AFFECTED BY THE WORK. THE CONDITION OF THE EXISTING CONDUITS AND THEIR APPURTENANCES SHALL BE DETERMINED FROM FIELD OBSERVATIONS. RECORDS OF THE INSPECTIONS SHALL BE KEPT BY THE VILLAGE OF ASHVILLE. ALL NEW CONDUITS,

**SUPPLEMENTAL NOTES (CONTINUED)**

**REVIEW OF DRAINAGE FACILITIES (CONTINUED)**

INLETS, AND MANHOLES CONSTRUCTED AS A PART OF THE PROJECT SHALL BE FREE OF ALL FOREIGN MATTER AND IN A CLEAN CONDITION BEFORE THE PROJECT WILL BE ACCEPTED BY THE VILLAGE OF ASHVILLE. ANY CHANGE IN THE CONDITION OF THE ABOVE DESCRIBED SEWERS AND APPURTENANCES RESULTING FROM THE CONTRACTOR'S OPERATIONS SHALL BE CORRECTED BY THE CONTRACTOR AND ANY COSTS SHALL BE INCLUDED IN THE CONTRACT PRICE FOR THE ASSOCIATED 901 PIPE ITEMS.

**PAVEMENT CUTS**

WHERE NECESSARY TO DISTURB PAVEMENTS OR DRIVES, THE PAVEMENT SHALL BE CUT TO FULL DEPTH OF PAVEMENT, OR AS DIRECTED BY THE ENGINEER, AND IN NEAT STRAIGHT LINES. IF, DURING CONSTRUCTION, THE PAVEMENT IS DAMAGED BEYOND THE ORIGINAL SAW CUT, THE PAVEMENT SHALL BE RECUT TO NEAT LINES. COST SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 448. THE ADDITIONAL PAVEMENT IN THESE AREAS NEEDED TO COMPLETE THE IMPROVEMENTS ACCORDING TO THESE PLANS SHALL BE REPLACED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE VILLAGE OF ASHVILLE FOR LABOR OR MATERIALS.

**RESTORATION OF DAMAGED FEATURES**

ALL SIGNS, FENCES, WALKS, SHRUBS, DRAINAGE STRUCTURES, OR OTHER PHYSICAL FEATURES THAT ARE TO REMAIN, WHICH ARE DISTURBED OR DAMAGED DUE TO THE CONTRACTOR'S NEGLIGENCE, SHALL BE RESTORED TO THEIR ORIGINAL CONDITION BY THE CONTRACTOR AT HIS OWN EXPENSE.

**CRACK SEALING, HOT APPLIED, TYPE II**

TRAFFIC SHALL NOT BE ALLOWED ON THE SEALANT UNTIL IT HAS CURED AND THE POSSIBILITY OF TRACKING DOES NOT EXIST. APPLIED SEALANT SHALL BE WETTED TO PREVENT TRACKING. ALL SEALANT APPLIED IN TEMPERATURES IN EXCESS OF 80 DEGREES FAHRENHEIT SHALL HAVE BLACK SLAG SAND APPLIED TO PREVENT CRACKING. THE SEALANT SHALL BE PLACED AT ALL CONSTRUCTION JOINTS. ALL COSTS TO BE INCLUDED IN ITEM 423 - CRACK SEALING, HOT APPLIED, TYPE II.

**STORM WATER POLLUTION PREVENTION PLAN, AS PER PLAN.**

CONTRACTOR TO PREPARE AND SUBMIT A STORM WATER POLLUTION PREVENTION PLAN (SWPPP) TO THE VILLAGE OF ASHVILLE ACCORDING TO ODOT SUPPLEMENTAL SPECIFICATION 832. SUBSTITUTE THE "VILLAGE OF ASHVILLE" FOR REFERENCES TO "ODOT" IN SS832. NO CONSTRUCTION IS PERMITTED PRIOR TO THE APPROVAL OF THE SWPPP.

ALL COSTS TO BE INCLUDED IN ITEM 832 - STORM WATER POLLUTION PREVENTION PLAN, AS PER PLAN, LUMP SUM.

**FARM AND BUILDING DRAINS**

ALL DRAINS, WHICH ARE ENCOUNTERED DURING CONSTRUCTION SHALL BE PROVIDED WITH UNOBSTRUCTED OUTLETS. EXISTING COLLECTORS WHICH ARE LOCATED BELOW THE ROADWAY DITCH ELEVATIONS, AND WHICH CROSS THE ROADWAY, SHALL BE REPLACED WITHIN THE CONSTRUCTION LIMITS BY ITEM 603 CONDUIT, TYPE B, ONE COMMERCIAL SIZE LARGER THAN THE EXISTING CONDUIT.

EXISTING COLLECTORS AND ISOLATED DRAINS, WHICH ARE ENCOUNTERED ABOVE THE ELEVATION OF ROADWAY DITCHES, SHALL BE OUTLETTED INTO THE ROADWAY DITCH BY 603 TYPE F CONDUIT. THE OPTIMUM OUTLET ELEVATION SHALL BE ONE FOOT ABOVE THE FLOWLINE ELEVATION OF THE DITCH. LATERAL FIELD TILES WHICH CROSS THE ROADWAY SHALL BE INTERCEPTED BY 603, TYPE E CONDUIT, AND CARRIED IN A LONGITUDINAL DIRECTION TO AN ADEQUATE OUTLET OR ROADWAY CROSSING.

THE LOCATION, TYPE, SIZE AND GRADE OF REPLACEMENTS SHALL BE DETERMINED BY THE ENGINEER AND PAYMENT SHALL BE MADE ON FINAL MEASUREMENTS.

EROSION CONTROL PADS AND ANIMAL GUARDS SHALL BE PROVIDED AT THE OUTLET END OF ALL DRAINS AS PER STANDARD CONSTRUCTION DRAWING DM-1.1, EXCEPT WHEN THEY OUTLET INTO A DRAINAGE STRUCTURE. PAYMENT FOR THE EROSION CONTROL PADS AND ANIMAL GUARDS AND ANY NECESSARY BENDS OR BRANCHES SHALL BE INCLUDED FOR PAYMENT IN THE PERTINENT CONDUIT ITEMS.

**SUPPLEMENTAL NOTES (CONTINUED)**

**FARM AND BUILDING DRAINS (CONTINUED)**

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE GENERAL SUMMARY FOR THE WORK NOTED ABOVE:

603 4" CONDUIT, TYPE F 20 FT.

603 6" CONDUIT, TYPE F 20 FT.

603 8" CONDUIT, TYPE B 20 FT.

603 12" CONDUIT, TYPE E 20 FT.

601 ROCK CHANNEL PROTECTION TYPE B WITH FABRIC FILTER 2 CU. YD.

**SIGNAL INSTALLATION**

THE CONTRACTOR SHOULD NOTE THAT SEVERAL TRAFFIC SIGNAL ITEMS ARE INSTALLED IN, UNDER, AND/OR ADJACENT TO THE PROPOSED SIDEWALK AND PAVEMENT. THESE ARE MOST EASILY INSTALLED IN COORDINATION WITH THE PROPOSED SIDEWALK AND PAVEMENT PLACEMENT. PAY ITEMS FOR CONDUIT INSTALLATION ARE FOR TRENCHING AND NOT DRILL/JACK/DIRECTIONAL BORE. IF DRILL/JACK/DIRECTIONAL BORE IS CHOSEN, THE ADDITIONAL COST WILL BE BORN BY THE CONTRACTOR. TRENCHING OF THE FINAL PAVEMENT COURSE OR PROPOSED SIDEWALKS WILL NOT BE ALLOWED.

**CONSTRUCTION NOISE**

ACTIVITIES AND LAND USE ADJACENT TO THIS PROJECT MAY BE AFFECTED BY CONSTRUCTION NOISE. IN ORDER TO MINIMIZE ANY ADVERSE CONSTRUCTION NOISE IMPACTS, DO NOT OPERATE POWER-OPERATED, CONSTRUCTION-TYPE DEVICES BETWEEN THE HOURS OF 7:00 P.M. AND 7:00 A.M.. IN ADDITION, DO NOT OPERATE AT ANY TIME ANY DEVICE IN SUCH A MANNER THAT THE NOISE CREATED SUBSTANTIALLY EXCEEDS THE NOISE CUSTOMARILY AND NECESSARILY ATTENDANT TO THE REASONABLE AND EFFICIENT PERFORMANCE OF SUCH EQUIPMENT.

**PROTECTION OF RIGHT-OF-WAY LANDSCAPING**

PRIOR TO BEGINNING WORK, THE CONTRACTOR, THE PROJECT ENGINEER, AND A REPRESENTATIVE OF THE MAINTAINING AGENCY WILL REVIEW AND RECORD ALL LANDSCAPING ITEMS WITHIN THE RIGHT OF WAY (BOTH WITHIN AND OUTSIDE THE CONSTRUCTION LIMITS) A RECORD OF THIS REVIEW WILL BE KEPT IN THE PROJECT ENGINEER'S FILES. PRIOR TO FINAL ACCEPTANCE, A FINAL REVIEW OF LANDSCAPING ITEMS WILL BE MADE.

CONSTRUCT ALL ACTIVITIES, EQUIPMENT STORAGE, AND STAGING TO WITHIN THE CONSTRUCTION LIMITS.

SUBMIT A WRITTEN REQUEST TO THE PROJECT ENGINEER TO USE ANY AREA OUTSIDE THESE LIMITS. THE DOCUMENT SUBMITTED MUST CLEARLY IDENTIFY THE AREA AND EXPLAIN THE PROPOSED USE AND RESTORATION OF THE AREA. USE OF THESE AREAS FOR DISPOSAL OF WASTE MATERIAL AND CONSTRUCTION DEBRIS, EXCAVATION OF BORROW MATERIAL AND PLACEMENT OF PORTABLE PLANTS IS PROHIBITED. THE REQUEST MUST BE APPROVED, IN WRITING, BEFORE THE CONTRACTOR HAS PERMISSION TO USE THE AREA.

ANY ITEMS DAMAGED BEYOND THE CONSTRUCTION LIMITS AS DEFINED ABOVE WILL BE REPLACED IN KIND OR AS APPROVED BY THE PROJECT ENGINEER AT THE CONTRACTOR'S EXPENSE.

CALCULATED  
WRA  
CHECKED  
BUS

GENERAL NOTES  
LONG STREET AND STATE ROUTE 752

PIC-752/LONG ST.

**SUPPLEMENTAL NOTES (CONTINUED)**  
**UNSUITABLE SOIL**

QUANTITIES FOR ITEM 203, EXCAVATION OF UNSUITABLE MATERIAL, ITEM 203 EMBANKMENT USING NO. 2 STONE, ITEM 203 EMBANKMENT USING NO. 57 STONE, ITEM 203 EMBANKMENT USING ITEM 304 AND ITEM SPECIAL GEOTEXTILE FABRIC HAVE BEEN INCLUDED FOR REMOVAL OF ANY UNSUITABLE SOIL ENCOUNTERED ON THIS PROJECT. THE ABOVE ITEMS, ESTABLISHED FOR THE REMOVAL AND REPLACEMENT OF UNSUITABLE SOIL SHALL BE USED ONLY AS DIRECTED BY THE ENGINEER AND SHALL BE ORDERED ONLY WITH THE PERMISSION OF THE ENGINEER.

THE GEOTEXTILE FABRIC SHALL BE TERRATEX HD, MIRAFI 600X, EXXON GTF300, OR AN APPROVED EQUAL HAVING A BURST STRENGTH OF AT LEAST 600 PSI.

**PROOF ROLLING**

THE FOLLOWING QUANTITY IS PROVIDED IN THE GENERAL SUMMARY TO ADDRESS LOCATIONS REQUIRING PROOF ROLLING AS DIRECTED BY THE ENGINEER.

ITEM 204 - PROOF ROLLING 8 HOURS.

**PROFILE AND ALIGNMENT**

PLACE THE PROPOSED PAVEMENT TO FOLLOW THE ALIGNMENT AND PROFILE OF THE EXISTING PAVEMENT. PLACE THE PROPOSED ASPHALT CONCRETE OVERLAY WITH A UNIFORM THICKNESS OF 1-1/4" INCHES.

**MANHOLES, CATCH BASINS AND INLETS REMOVED OR ABANDONED**

ALL CASTINGS SHALL BE CAREFULLY REMOVED AND STORED WITHIN THE RIGHT OF WAY FOR SALVAGE BY VILLAGE FORCES.

PAYMENT FOR ALL OF THE ABOVE SHALL BE INCLUDED IN THE CONTRACT PRICE FOR THE PERTINENT 202 ITEM.

**EXISTING UNDERDRAINS**

PROVIDE UNOBSTRUCTED OUTLETS FOR ALL EXISTING UNDERDRAINS ENCOUNTERED DURING CONSTRUCTION.

PROVIDE AN OUTLET PER STANDARD CONSTRUCTION DRAWING DM-1.1 FOR ALL UNDERDRAINS THAT OUTLET TO A SLOPE.

UNDERDRAINS THAT CAN BE CONNECTED TO THE NEW OR EXISTING UNDERDRAINS AT THE END OF THE PROJECT LIMITS AS WELL AS ALL NECESSARY BENDS OR BRANCHES REQUIRED FOR CONNECTION ARE INCLUDED IN THE BASIS OF PAYMENT FOR UNCLASSIFIED PIPE UNDERDRAINS.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE GENERAL SUMMARY FOR THE WORK NOTED ABOVE:

- 603 - 6" CONDUIT, TYPE F 20 FT.
- 604 - PRECAST REINFORCED CONCRETE OUTLET 2 EACH
- 605 - 8" UNCLASSIFIED PIPE UNDERDRAINS 20 FT.

**CONTAMINATED MATERIAL REMOVED AND DISPOSED OF**

CONTAMINATED SOILS MAY EXIST ON THE SERVICE STATION PROPERTY LOCATED AT THE NORTHEAST CORNER OF THE INTERSECTION. ANY EXCAVATED MATERIALS THAT ARE CONTAMINATED SHALL BE DISPOSED OF AT A LICENSED HAZARDOUS WASTE DISPOSAL FACILITY THAT HAS EITHER OHIO OR U.S. EPA APPROVAL TO HANDLE THE CONTAMINATED MATERIAL. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL TESTING TO DETERMINE THE NATURE OF THE CONTAMINATION AND AS REQUIRED BY THE FACILITY ACCEPTING THE CONTAMINATED MATERIAL.

THE CONTRACTOR SHALL UTILIZE CURRENT SAFETY PROCEDURES TO INSURE SAFETY OF HIS EMPLOYEES AND THE PUBLIC FROM EXPOSURE TO ANY HAZARDOUS MATERIALS THAT MAY BE ENCOUNTERED.

**SUPPLEMENTAL NOTES (CONTINUED)**  
**CONTAMINATED MATERIAL REMOVED AND DISPOSED OF (CONTINUED)**

ALL ASSOCIATED COSTS FOR TESTING, HANDLING AND DISPOSING OF THE CONTAMINATED MATERIAL SHALL BE INCLUDED IN THE UNIT BID PRICE FOR ITEM SPECIAL, CONTAMINATED MATERIAL REMOVED AND DISPOSED OF. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN PROVIDED IN THE GENERAL SUMMARY FOR THIS PURPOSE.

ITEM SPECIAL - CONTAMINATED SOIL REMOVED AND DISPOSED OF 20 CU. YD.

**ROADWAY NOTES**

**RDW 1** ALL PAVEMENT SUB-GRADE SHALL BE CONSTRUCTED IN ACCORDANCE WITH ITEM 203 OF THE CITY OF COLUMBUS CONSTRUCTION AND MATERIAL SPECIFICATIONS. SECTION 203.12 SHALL BE MODIFIED SUCH THAT ALL COMPACTION SHALL BE TO ONE HUNDRED (100) PERCENT OF THE MAXIMUM DRY UNIT WEIGHT OBTAINED IN THE LABORATORY BY THE "STANDARD PROCTOR" COMPACTION TEST (ASTM D698). MOISTURE CONTENT OF THE NEW FILL SHALL BE IN RANGE OF +/- TWO (2) PERCENT OF THE OPTIMUM MOISTURE CONTENT DETERMINED BY ASTM D698.

**RDW 2** PRIOR TO CONSTRUCTION OF THE STREETS, SOIL TESTS SHALL BE MADE ON ALL DESIGNATED STORM SEWER TRENCHES WHICH CROSS PROPOSED PAVEMENTS OR WHICH LIE SUCH THAT THE PROPOSED PAVEMENTS ARE LOCATED WITHIN ANY PART OF THE INFLUENCE LINE OF SAID TRENCH. WHERE SAID RESULTS INDICATE THAT THE TRENCH BACKFILL DOES NOT MEET THE COMPACTION REQUIREMENTS OF 912.03 (MINIMUM OF 100 % OF MAXIMUM LABORATORY DENSITY) OF THE CONSTRUCTION AND MATERIAL SPECIFICATIONS, ALL BACKFILL MATERIAL SHALL BE REMOVED, REPLACED, AND RE-TESTED UNTIL COMPACTION MEETS SAID REQUIREMENTS OF 912.03.

**RDW 3** ALL PAVEMENT JOINTS, PARTICULARLY WHERE A PROPOSED PAVEMENT ABUTS AN EXISTING PAVEMENT, AND ALL PAVEMENT JOINTS ABUTTING UTILITY STRUCTURES SUCH AS MANHOLES, CATCH BASINS, VALVE BOXES, ETC. SHALL BE SEALED IN ACCORDANCE WITH ITEM 413 TYPE (1) OF THE CITY OF COLUMBUS CONSTRUCTION AND MATERIAL SPECIFICATIONS.

**RDW 7** PAVEMENT CUTS FOR UTILITY LINE INSTALLATIONS ARE SUBJECT TO THE BACKFILL REQUIREMENTS OF ITEM 912. IN LIEU OF COMPACTED GRANULAR MATERIAL, FLOWABLE CONTROLLED DENSITY FILL, ITEM 636 TYPE-II MAY BE USED. PAVEMENT SHALL BE CONSTRUCTED TO MATCH THE EXISTING SECTION OR NINE INCHES OF ITEM 404 ASPHALT CONCRETE, WHICHEVER IS GREATER. AS AN ALTERNATE, A 7" THICK CONCRETE BASE, EXTENDING ONE (1) FOOT BEYOND EITHER EDGE OF THE EXCAVATION MAY BE USED, WITH TWO (2) INCHES OF ITEM 404 ASPHALT WEARING COURSE PLACED ON TOP.

**RDW 8** STEEL PLATES SHALL BE POSITIONED AND SECURED IN PLACE WITH STEEL SPIKES AND COLD PATCH ASPHALT MIX OVER ALL TRENCHES THAT ARE LEFT OPEN ON A TEMPORARY BASIS AND SUBJECT TO TRAFFIC.

**RDW 9** VILLAGE STREETS ARE TO BE KEPT CLEAN AND FREE FROM MUD, STONE, DIRT, ETC. A TEMPORARY CONSTRUCTION ENTRANCE COMPRISED OF A 20' X 50' MAT OF NUMBER 2 STONE IS TO BE MAINTAINED AT ALL SITE ENTRANCES.

**ROADWAY NOTES (CONTINUED)**

**RDW 10** CONCRETE CURBS ARE TO BE BRANDED DURING PLACEMENT UTILIZING THE STANDARD BRAND SET PROVIDED BUY THE VILLAGE OF ASHVILLE ENGINEER. BRAND CURBS ARE AS FOLLOWS:

- "S" - ON TOP OF CURB FOR SANITARY LATERAL LOCATIONS
- "W" - ON FACE OF CURB FOR WATER SERVICE BOX LOCATIONS.
- "WV" - ON FACE OF CURB FOR HYDRANT WATCH VALVE LOCATIONS.
- "WM" - ON FACE OF CURB FOR WATER MAIN VALVE LOCATIONS.

BRANDS THAT ARE MISSED MUST BE MECHANICALLY GROUNDED INTO CURB AFTER THE CONCRETE IS SET.

**RDW 11** THE COMBINATION CURB AND GUTTER SHALL BE PLACED CONTINUOUSLY.

**RDW 12** ITEM 604, REFERENCE MONUMENT, AS PER PLAN. THE CONTRACTOR SHALL INSTALL REFERENCE MONUMENTS & MONUMENT BOXES AS INDICATED ON THE PLANS, AS PER OHIO DEPARTMENT OF TRANSPORTATION, STANDARD CONSTRUCTION DRAWING RM-1.1, DATED 1-21-11, TYPE C. THE FOLLOWING QUANTITY HAS BEEN INCLUDED IN THE GENERAL SUMMARY.

ITEM 604 REFERENCE MONUMENT, AS PER PLAN 1 EA

**RDW 13** ALL TRAFFIC CONTROL DEVICES SHALL BE FURNISHED, ERECTED, MAINTAINED, AND REMOVED BY THE CONTRACTOR IN ACCORDANCE WITH THE "OHIO MANUAL OF TRAFFIC CONTROL DEVICES FOR CONSTRUCTION AND MAINTENANCE OPERATIONS" COPIES OF WHICH ARE AVAILABLE FROM THE OHIO DEPARTMENT OF TRANSPORTATION, BUREAU OF TRAFFIC, 25 SOUTH FRONT STREET, COLUMBUS, OHIO 43215.

**RDW 14** LANE RESTRICTIONS OR CLOSURES REQUIRED DURING CONSTRUCTION MUST BE APPROVED BY THE VILLAGE ENGINEER AND SERVICE DEPARTMENTS A MINIMUM OF TWO (2) WEEKS PRIOR TO ANY WORK BEING PERFORMED. OTHERWISE, TRAFFIC LANES SHALL BE FULLY OPEN TO TRAFFIC AT ALL TIMES AND INGRESS AND EGRESS SHALL BE MAINTAINED TO PUBLIC AND PRIVATE PROPERTY.

**RDW 15** THE CONTRACTOR IS RESPONSIBLE FOR THE PROVISION AND SCHEDULING OF PERSONNEL FOR CONCRETE, ASPHALT, AND SOILS TESTING PROCEDURES AS REQUIRED BY THE VILLAGE ENGINEER. TESTING IS TO BE PERFORMED BY A REGISTERED TESTING AGENCY THAT IS APPROVED BY THE VILLAGE ENGINEER.

**RDW 16** THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR THE PHYSICAL CONSTRUCTION ONLY. THE INSTALLATION AND OPERATION OF ALL TEMPORARY TRAFFIC CONTROL DEVICES SHALL BE PROVIDED BY THE CONTRACTOR WHETHER INSIDE OR OUTSIDE OF THE WORK LIMITS, IN ACCORDANCE WITH THE OHIO MANUAL OF TRAFFIC CONTROL SERVICES FOR CONSTRUCTION AND MAINTENANCE OPERATIONS (CURRENT EDITION), COPIES OF WHICH ARE AVAILABLE FROM THE OHIO DEPARTMENT OF TRANSPORTATION, OFFICE OF TRAFFIC ENGINEERING.

**RDW 17** THE TRACKING OR SPILLAGE OF MUD, DIRT OR DEBRIS UPON VILLAGE STREETS IS PROHIBITED AND ANY SUCH OCCURRENCE SHALL BE CLEANED UP IMMEDIATELY BY THE CONTRACTOR.

**RDW 18** NO NON-RUBBER TIRE VEHICLE SHALL BE MOVED ON VILLAGE STREETS. EXCEPTIONS MAY BE GRANTED BY AN AUTHORIZED VILLAGE OFFICIAL WHERE SHORT DISTANCES AND SPECIAL CIRCUMSTANCES ARE INVOLVED. GRANTING OF EXCEPTIONS MUST BE IN WRITING AND ANY RESULTING DAMAGE MUST BE REPAIRED TO THE SATISFACTION OF THE VILLAGE.

**ROADWAY NOTES (CONTINUED)**

**RDW 19** STEADY BURNING TYPE "C" LIGHTS SHALL BE REQUIRED ON ALL BARRICADES, DRUMS AND SIMILAR TRAFFIC DEVICES IN USE AT NIGHT. CONES ARE NOT PERMITTED TO BE USED FOR NIGHT WORK.

**RDW 20** ALL TRAFFIC LANES SHALL BE FULLY OPEN TO TRAFFIC FROM 7:00 A.M. TO 9:00 A.M. AND 4:00 P.M. TO 6:00 P.M., UNLESS OTHERWISE APPROVED BY THE VILLAGE. ONE LANE MAY BE CLOSED TO TRAFFIC DURING WORKING HOURS.

**RDW 21** TEMPORARY PAVEMENT REPLACEMENT SHALL BE PROVIDED ON PERMANENT PAVEMENTS DAMAGED OR REMOVED BY THE CONTRACTOR IN THE PERFORMANCE OF THE WORK TO LIMITS SHOWN ON THE PLANS OR ORDERED BY THE ENGINEER. AS SOON AS THE TRENCH HAS BEEN BACKFILLED, TEMPORARY PAVEMENT SHALL BE INSTALLED. THE ENGINEER MAY REQUIRE THAT ALL MATERIALS AND EQUIPMENT INCIDENTAL TO PROVIDING THE TEMPORARY PAVEMENT BE ON THE JOB SITE PRIOR TO REMOVING THE EXISTING PAVEMENT. THE TEMPORARY PAVEMENT SHALL CONSIST OF 2 INCHES OF COMPACTED BITUMINOUS MATERIAL ITEM 301 CMSC PLACED UPON 6 INCHES OF COMPACTED ITEM 304, CMSC AGGREGATE BASE. TEMPORARY PAVEMENT SHALL BE MAINTAINED BY THE CONTRACTOR UNTIL PERMANENT PAVEMENT IS INSTALLED.

**RDW 22** THE TEMPORARY PAVEMENT SHALL BE REPLACED BY FIRST REMOVING THE TEMPORARY PAVEMENT DOWN TO CLEAN GRANULAR MATERIAL AND REMOVING THE EXISTING PAVEMENT FOR AT LEAST 12 INCHES BEYOND THE TRENCH LIMITS ON EACH SIDE. THE PAVEMENT TO BE REMOVED SHALL BE NEATLY SAWCUT TO A DEPTH SPECIFIED BY THE ENGINEER, NOT MORE THAN 24 HOURS PRIOR TO PLACING THE PERMANENT PAVEMENT MATERIALS. THE PERMANENT PAVEMENT MATERIALS AND WORKMANSHIP SHALL BE AT LEAST EQUIVALENT TO THE EXISTING, ADJACENT PAVEMENT, AS DETERMINED BY THE ENGINEER. AFTER REMOVAL OF THE PAVEMENT MATERIAL, ITEM 407, TACK COAT, SHALL BE APPLIED TO THE EXPOSED EXISTING PAVEMENT EDGES, AND ITEM 408, PRIME COAT, SHALL BE APPLIED TO THE BASE MATERIAL PRIOR TO THE PLACING OF THE PERMANENT PAVEMENT. ITEM 413, CRACK SEALING SHALL BE APPLIED ALONG EACH EDGE.

**RDW 23** ALL TRENCHES IN THE ROADWAY SHALL BE BACKFILLED OR SECURELY PLATED DURING NON-WORKING HOURS.

**RDW 24** IN THE EVENT EXCAVATION FOR THE STREET IS FROM ZERO (0) INCHES TO TWELVE (12) INCHES BELOW WHAT IS CALLED FOR ON THE PLANS, THE CONTRACTOR SHALL REPLACE THIS EXCESS EXCAVATED MATERIAL AS DIRECTED BY A VILLAGE ENGINEER USING CMSC ITEM 304 AS DIRECTED. A CONTINGENCY QUANTITY OF 20 CUBIC YARDS OF ITEM 304 HAS BEEN ADDED TO THE QUANTITIES FOR THIS PURPOSE.

**RDW 25** WHERE NECESSARY TO DISTURB EXISTING PAVEMENT OR DRIVES, THE PAVEMENT SHALL BE SAW CUT IN NEAT, STRAIGHT LINES. THE DEPTH OF SAW CUT SHALL BE FULL DEPTH OR AS DIRECTED BY THE ENGINEER.

**STORMWATER NOTES**

**STM 1** THE MINIMUM REQUIREMENTS FOR STORM SEWER PIPE WITHIN THE VILLAGE RIGHT-OF-WAY OR EASEMENTS SHALL BE REINFORCED CONCRETE PIPE ASTM C655 OR ASTM C76 AND NON-REINFORCED CONCRETE PIPE ASTM C14 OR CORRUGATED POLYETHYLENE SMOOTH LINED PIPE, M-294, TYPES S, AS PER OHIO DEPARTMENT OF TRANSPORTATION CONSTRUCTION AND MATERIAL SPECIFICATIONS ITEM 707.

**STORMWATER NOTES (CONTINUED)**

**STM 2** FLEXIBLE STORM SEWERS ARE SUBJECT TO MANDREL TESTING AND/OR VIDEO INSPECTION AS DIRECTED BY THE VILLAGE ENGINEER. TESTING SHALL BE PERFORMED NO SOONER THAN THIRTY (30) DAYS AFTER THE PIPE TRENCH HAS BEEN BACKFILLED AND ALL ROADWAY AND SITE FILLS OVER THE STORM LINES HAVE BEEN CONSTRUCTED. MAXIMUM DEFLECTION SHALL NOT EXCEED 7.5% OF THE BASE INSIDE DIAMETER. COST OF TESTING SHALL BE AT THE EXPENSE OF THE CONTRACTOR.

**STM 5** THE COST OF ANY ROCK EXCAVATION SHALL BE INCLUDED IN THE PRICE BID FOR THE STORM SEWER. THE BIDDER SHALL DETERMINE IF ANY ROCK EXCAVATION WILL BE REQUIRED AND ADJUST THEIR BIDS ACCORDINGLY.

**STM 6** THE FLOW IN ALL SEWERS, DRAINS, AND WATERCOURSES ENCOUNTERED SHALL BE MAINTAINED BY THE CONTRACTOR AT THEIR OWN EXPENSE AND WHENEVER SUCH WATERCOURSES AND DRAINS ARE DISTURBED OR DESTROYED AS A RESULT OF THIS CONSTRUCTION SHALL BE RESTORED BY THE CONTRACTOR AT HIS OWN EXPENSE TO A CONDITION SATISFACTORY TO THE ENGINEER.

**STM 8** EROSION CONTROL MEASURES ARE TO BE INSTALLED BY THE CONTRACTOR DURING CONSTRUCTION TO PROTECT MANHOLES, CATCH BASINS AND CURB INLETS FROM SILT, MUD, AND DEBRIS.

**STM 10** ALL CATCH BASINS, MANHOLES, AND CURB INLETS SHALL HAVE CONCRETE CHANNELS POURED IN PLACE TO ASSURE POSITIVE DRAINAGE THROUGH THESE STRUCTURES.

**STM 11** PUBLIC STORM SEWER MANHOLE LIDS ARE TO BE EAST JORDAN IRON WORKS NUMBER 1660 - A2 OR EQUIVALENT AND EMBOSSED "VILLAGE OF ASHVILLE STORM SEWER".

**STM 12** STORM SEWER CURB INLETS ARE TO BE ADJUSTED WITHIN 1/4" OF PLAN ELEVATION USING STEEL SHIMS.

**STM 13** PRE-CAST RINGS ARE TO BE USED FOR ALL FINAL ADJUSTMENTS OF MANHOLE CASTINGS.

**STM 14** OPENINGS SHALL BE PROVIDED IN DRAINAGE STRUCTURES TO ACCOMMODATE UNDERDRAIN OUTLETS. UNDERDRAINS ARE TO BE CONSTRUCTED IN ACCORDANCE WITH DETAILS GIVEN IN THE PLANS.

**SEWER NOTES**

**SAS 1** ALL SEWERS, APPURTENANCES, AND METHODS OF CONSTRUCTION AND WORKMANSHIP FOR SEWERS AND APPURTENANCES SHOWN ON THESE PLANS SHALL CONFORM TO THE CITY OF COLUMBUS CONSTRUCTION AND MATERIAL SPECIFICATION SECTION 900 AND APPLICABLE REFERENCES THEREIN, CURRENT ON THE DATE OF THE CONTRACT, UNLESS THE REQUIREMENTS OF SUCH RULES AND REGULATIONS ARE UPGRADED OR MODIFIED BY THE FOLLOWING NOTES OR BY THE CONSTRUCTION DETAILS SET FORTH HEREIN.

**SAS 4** CLEAN WATER CONNECTIONS INCLUDING ROOF DRAINS, FOUNDATION DRAINS, SUMPS, ETC. ARE PROHIBITED FROM BEING CONNECTED TO THE SANITARY SEWER.

**SAS 11** PUBLIC SANITARY MANHOLE COVERS ARE TO BE EAST JORDAN IRON WORKS NO. 1660-A2 OR EQUIVALENT AND EMBOSSED "VILLAGE OF ASHVILLE SANITARY SEWER".

**SAS 15** PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL VERIFY MANHOLE CONSTRUCTION AND TOP-OF-CASTING ELEVATION. MANHOLES SHALL BE BUILT OR ADJUSTED SO THE TOPS CONFORM TO THE ELEVATIONS SHOWN ON THESE PLANS. ALL MANHOLE CASTING ADJUSTMENTS SHALL BE ACCOMPLISHED WITH PRE-CAST CONCRETE ADJUSTMENT RINGS.

**SAS 20** A PERMANENT FLEXIBLE WATER JOINT SEALANT BETWEEN THE TOPS OF SANITARY MANHOLES AND MANHOLE CASTINGS SHALL BE INSTALLED. SEALANT TO BE "CONSEAL" AS MANUFACTURED BY CONCRETE SEALANTS, INC. NEW CARLISLE, OHIO OR APPROVED EQUAL.

**SEWER NOTES (CONTINUED)**

**SAS 21** EACH MANHOLE SHALL BE PROVIDED WITH A "CHIMNEY SEAL" CONSISTING OF A TRIPLE PLEATED RUBBER SLEEVE HAVING A MINIMUM THICKNESS OF 3/16 INCH, A MINIMUM UNEXPANDED VERTICAL HEIGHT OF EIGHT (8) INCHES, CAPABLE OF EXPANDING NOT LESS THAN TWO (2) INCHES VERTICALLY WHEN INSTALLED, EQUIPPED WITH SIXTEEN (16) GAUGE BY 1-3/4 INCH WIDE A.S.T.M. STAINLESS STEEL TYPE 304 EXPANSION BANDS, THE ENTIRE UNIT AS MANUFACTURED BY CRETEX SPECIALTY PRODUCTS OR APPROVED EQUAL

**SANITARY AND STORMWATER SEWER NOTES**

**STM/SAS 2** ALL SEWERS CONSTRUCTED UNDER THIS PLAN SHALL MEET THE REQUIREMENTS OF THE VILLAGE OF ASHVILLE, UNLESS OTHERWISE NOTED.

**STM/SAS 16** MANHOLE COVERS FOR SANITARY SEWERS SHALL CONFORM TO VILLAGE OF ASHVILLE STANDARD DRAWINGS.

**STM/SAS 17** MANHOLE TOPS SHALL BE BUILT OR SUBSEQUENTLY ADJUSTED TO MEET SURFACE GRADES ESTABLISHED. COST OF THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR THE VARIOUS SEWER ITEMS.

**STM/SAS 20** ALL NEW SANITARY SEWER MANHOLES AND EXISTING SANITARY MANHOLES ADJUSTED TO GRADE SHALL BE CONSTRUCTED WITH CHIMNEY SEALS AND DISH IN ACCORDANCE WITH VILLAGE OF ASHVILLE'S SPECIFICATIONS.

**STM/SAS 21** ALL NEW MANHOLES AND EXISTING MANHOLES ADJUSTED TO GRADE SHALL HAVE CONCRETE COLLARS.

**STM/SAS 25** THE CONTRACTOR SHALL MAKE PROVISIONS TO MAINTAIN FLOWS IN THE EXISTING SEWER AT ALL TIMES DURING CONSTRUCTION.

**STM/SAS 30** THE CONTRACTOR SHALL SUBMIT TO THE VILLAGE ENGINEER FOR HIS OR HER REVIEW, FIVE COPIES OF SHOP DRAWINGS FOR ALL MATERIALS, STRUCTURES, GRADATION CERTIFICATIONS AND EQUIPMENT BEFORE ANY OF THE SAID MATERIALS, STRUCTURES OR EQUIPMENT IS ORDERED. THE VILLAGE DOES NOT BARE ANY RESPONSIBILITY TO ACCEPT ANY OF THE ABOVE MENTIONED ITEMS WITHOUT A COMPLETED REVIEW OF SAID SHOP DRAWINGS. THESE DOCUMENTS SHALL BE BOUND AND INDEXED WITH NUMBERED SHEETS FOR EACH ITEM OR PAGES NUMBERED THROUGHOUT.

**WATER MAIN/LINE NOTES**

**WML 1** ALL WATERLINE SERVICE SADDLES SHALL BE DOUBLE BOLT STAINLESS STEEL STYLE 306 AS MANUFACTURED BY ROMAC, OR EQUAL.

**WML 2** LOCATION, SUPPORT, PROTECTION AND RESTORATION OF ALL WATER LINES, SERVICES AND APPURTENANCES SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. THE CONTRACTOR SHALL SUBMIT HIS DETAILS AND METHODS OF SUPPORTING THE WATER LINES ACROSS THE SEWER TRENCH TO THE ENGINEER FOR APPROVAL BY THE WATER DEPARTMENT. SUPPORT METHOD AND DETAIL APPROVAL SHALL BE SECURED PRIOR TO THE COMMENCEMENT OF EXCAVATION OPERATIONS.

**WML 3** WATER MAINS SHALL, WHERE POSSIBLE, BE DEFLECTED AROUND STRUCTURES WITHOUT THE USE OF SPECIAL FITTINGS AND WITHOUT EXCEEDING THE MANUFACTURER'S ALLOWABLE DEFLECTION.

**WML 4** THE CONTRACTOR SHALL HAND SWAB ALL PIPE AND FITTINGS THAT ARE NOT OTHERWISE DISINFECTED. THE AMOUNT OF CHLORINE TO BE USED DURING HAND SWABBING OPERATIONS SHALL BE DETERMINED BY THE VILLAGE OF ASHVILLE, WATER DEPARTMENT.

**WATER MAIN/LINE NOTES (CONTINUED)**

**WML 5** ANY TESTING PERFORMED AGAINST EXISTING VALVES SHALL BE DONE AT THE CONTRACTOR'S RISK AND IN STRICT COMPLIANCE WITH THE REQUIREMENTS OF THE ENGINEER. IF SATISFACTORY TEST RESULTS CANNOT BE OBTAINED AGAINST AN EXISTING VALVE, THE NEW LINE SHALL BE DISCONNECTED FROM THE EXISTING LINE, PLUGGED AND RE-TESTED. DAMAGE CAUSED TO EXISTING LINES AND VALVES SHALL BE REPAIRED BY THE CONTRACTOR AT HIS OWN EXPENSE.

**WML 6** ALL COST TO PLUG AND BLOCK THE ENDS OF WATER MAINS AT LOCATIONS SHOWN IN THE PLANS SHALL BE INCLUDED IN THE PRICE BID FOR PIPE.

**WML 7** WATER SERVICE BOXES SHALL BE LOCATED 6" TO 12" FROM THE ROADWAY RIGHT-OF-WAY LINE, EXCEPT AS SHOWN ON THE PLANS.

**WML 8** A DETECTABLE UNDERGROUND UTILITY MARKING TAPE SHALL BE INSTALLED APPROXIMATELY 18" BELOW GRADE. THIS TAPE SHALL CONSIST OF A MINIMUM 5 MIL OVERALL THICKNESS, WITH A SOLID ALUMINUM FOIL CORE. A 2 MIL CLEAR FILM REVERSE PRINT LAMINATED TO AN ALUMINUM FOIL TO 2 MIL CLEAR FILM, MAKING THE FILM PERMANENTLY PRINTED. COLOR CODE SHALL BE BLUE INDICATING WATER AND ASSOCIATED LINES. THE TAPE SHALL BE MANUFACTURED TO WITHSTAND ALKALINE, ACIDIC AND NEUTRAL SOIL CONDITIONS. ANY DIRECTIONAL BORED PIPE SHALL HAVE A NO. 6 BRAIDED WIRE, INSTALLED WITH THE PIPE. COST SHALL BE INCLUDED WITH THE PRICE OF PIPE. NO SEPARATE PAYMENT WILL BE MADE.

**WML 9** THE CONTRACTOR SHALL SUBMIT TO THE VILLAGE ENGINEER FOR REVIEW, FIVE COPIES OF SHOP DRAWINGS FOR ALL MATERIALS, STRUCTURES, GRADATION CERTIFICATIONS AND EQUIPMENT BEFORE ANY OF THE SAID MATERIALS, STRUCTURES AND EQUIPMENT IS ORDERED. THE VILLAGE OF ASHVILLE DOES NOT BEAR ANY RESPONSIBILITY TO ACCEPT ANY OF THE ABOVE-MENTIONED ITEMS WITHOUT A COMPLETE REVIEW OF SAID SHOP DRAWINGS. THESE DOCUMENTS SHALL BE BOUND INTO A FOLDER WITH EITHER AN INDEX WITH NUMBERED SHEETS FOR EACH ITEM OR PAGES IDENTIFIED THROUGHOUT.

**WML 10** ALL WATER PIPE AND FITTINGS, AND METHODS OF CONSTRUCTION AND WORKMANSHIP FOR WATER LINES AND APPURTENANCES SHOWN AN THESE PLANS SHALL CONFORM TO THE CITY OF COLUMBUS CONSTRUCTION AND MATERIAL SPECIFICATIONS, SECTION 800 AND APPLICABLE REFERENCES THEREIN, CURRENT ON THE DATE OF CONTRACT, UNLESS THE REQUIREMENTS OF SUCH RULES AND REGULATIONS ARE UPGRADED OR MODIFIED BY THE FOLLOWING NOTES OR BY THE CONSTRUCTION DETAILS SET FORTH HEREIN.

**WML 11** WORK REQUIRING THE SHUTDOWN OF EXISTING WATER MAINS IS TO BE COORDINATED WITH THE VILLAGE OF ASHVILLE WATER DEPARTMENT FORTY-EIGHT (48) HOURS PRIOR TO THE SCHEDULED WORK BEING PERFORMED. ALL AFFECTED CUSTOMERS SHALL BE NOTIFIED BY THE CONTRACTOR AT LEAST TWENTY-FOUR (24) HOURS PRIOR TO THE SHUT DOWN.

**WML 12** WATER MAINS SHALL BE DUCTILE IRON PIPE DESIGNED IN ACCORDANCE WITH THE LATEST REVISIONS OF A.N.S.I./A.W.W.A. C150/A21.50 FOR A MINIMUM 150 PSI (OR PROJECT REQUIREMENTS, WHICHEVER IS GREATER) RATED WORKING PRESSURE PLUS A 100 PSI MINIMUM SURGE ALLOWANCE; A 2 TO 1 FACTOR OF SAFETY ON THE SUM OF WORKING PRESSURE PLUS SURGE PRESSURE.

**WML 13** WATER MAINS SHALL BE DUCTILE IRON PIPE, CLASS 52 (AWWA C151) WITH CEMENT MORTAR LINING AND SEAL COATING (AWWA C104) IN ACCORDANCE WITH VILLAGE SPECIFICATIONS. JOINTS SHALL BE RUBBER GASKET PUSH-ON MECHANICAL TYPE (AWWA C111). WATER MAIN FITTINGS SHALL BE DUCTILE IRON WITH CEMENT MORTAR LINING AND SEAL COATING WITH MECHANICAL JOINTS AND SHALL CONFORM TO AWWA C153.

**WATER MAIN/LINE NOTES (CONTINUED)**

**WML 14** DUCTILE IRON PIPE SHALL BE MANUFACTURED IN THE U.S.A. IN ACCORDANCE WITH THE LATEST REVISION OF A.N.S.I./A.W.W.A. C151/A21.51. EACH PIPE SHALL BE SUBJECT TO A HYDROSTATIC PRESSURE TEST OF AT LEAST 500 PSI AT THE POINT OF MANUFACTURE.

**WML 15** PIPE SHALL HAVE STANDARD ASPHALTIC COATING ON THE EXTERIOR PIPE AND SHALL ALSO HAVE A CEMENT MORTAR ON THE INTERIOR IN ACCORDANCE WITH A.N.S.I./A.W.W.A. C104/A21.4 OF THE LATEST REVISION.

**WML 16** THE CLASS OR NOMINAL THICKNESS, NET WEIGHT WITHOUT LINING, AND CASTING DATE SHALL BE CLEARLY MARKED ON EACH LENGTH OF PIPE. ADDITIONALLY, THE MANUFACTURER'S MARK, COUNTRY WHERE CAST, YEAR IN WHICH THE PIPE WAS PRODUCED, AND LETTERS "DI" OR "DUCTILE" SHALL BE CAST OR STAMPED ON THE PIPE.

**WML 17** PVC PLASTIC PIPE, A.W.W.A. C900 DR 18 FOR SIZES 4" TO 12" AND A.W.W.A. C905 DR 18 FOR SIZES 14" AND ABOVE MAY BE USED ONLY WHEN APPROVED BY THE VILLAGE OF ASHVILLE.

**WML 19** DEAD END WATER LINES SHALL TERMINATE WITH A FIRE HYDRANT AND A WATCH VALVE OR TWO 3/4 INCH WATER SERVICES FOLLOWED BY SUFFICIENT LENGTH OF WATER LINE TO RESTRAIN THE VALVE AND SHALL BE FOLLOWED BY A MAIN LINE VALVE AND AN ADDITIONAL SECTION OF WATER LINE PLUGGED AND BLOCKED PER THE STANDARD DRAWINGS.

**WML 20** WATERLINES SHALL BE INSTALLED WITH A MINIMUM OF FOUR (4) FEET OF COVER, MEASURED FROM FINISHED GRADE TO THE TOP OF THE WATER MAIN.

**WML 22** THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE HORIZONTAL AND VERTICAL DEFLECTIONS OR BENDS OF THE WATER LINES IN ACCORDANCE WITH THE MANUFACTURER'S SPECIFICATIONS. WHERE NOTED ON THE PLANS, DEFLECT WATER LINES TO PROVIDE 1'-6" VERTICAL AND 10'-0" HORIZONTAL CLEARANCE FROM SANITARY AND STORM SEWERS.

**WML 23** IF THERE ARE ANY CONFLICTS IN GRADE BETWEEN WATER LINES AND GRAVITY SEWERS, THE WATER LINES SHALL BE LOWERED DURING CONSTRUCTION. CLEARANCES STATED MUST BE MAINTAINED.

**WML 26** THE CAPS AND INSIDE OF ALL MAINLINE WATER VALVE BOXES SHALL BE PAINTED BLUE, AND THE CAPS AND INSIDE OF ALL FIRE HYDRANT GATE VALVE BOXES SHALL BE POINTED RED WITH TWO COATS OF RUST INHIBITIVE PAINT. PUBLIC FIRE HYDRANTS ARE TO BE PAINTED WITH TWO COATS OF FEDERAL SAFETY RED, SHERWIN WILLIAMS B54Y37 PER VILLAGE OF ASHVILLE 1507.08(A)(1). PRIVATE FIRE HYDRANTS ARE TO BE PAINTED FEDERAL SAFETY RED AND WHITE CAPS AND BONNETS PER VILLAGE OF ASHVILLE 1507.14(B)(4). HYDRANTS PLACES IN A FACTORY LOOPED SYSTEM SHALL BE PAINTED ALL FEDERAL SAFETY RED.

**WML 27** ALL MECHANICAL FASTENERS, BOLTS, ALL THREAD ROD, ETC., ARE TO RECEIVE ONE COAT OF RUST INHIBITIVE POINT OR COATING.

**WML 28** IF THE TOP OF THE OPERATING NUT IS MORE THAN THIRTY SIX (36) INCHES BELOW FINISHED GRADE, AN EXTENSION STEM SHALL BE FURNISHED TO BRING THE TOP OF THE OPERATING NUT TO WITHIN TWENTY-FOUR (24) INCHES OF FINISHED GRADE ELEVATION. ALL VALVE STEM EXTENSIONS ARE TO BE INSTALLED WITH THE EXTENSION SECURED DIRECTLY TO THE VALVE OPERATING NUT WITH GALVANIZED RESTRAINT CLIPS.

**WML 29** THE COST OF ANY DEWATERING OPERATIONS REQUIRED FOR THE CONSTRUCTION OF THE WATER LINE SHALL BE INCLUDED IN THE PRICE BID FOR THE VARIOUS ITEMS.

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WRA  
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GENERAL NOTES  
LONG STREET AND STATE ROUTE 752

PIC-752/LONG ST.

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**WATER MAIN/LINE NOTES (CONTINUED)**

**WML 30** ALL FITTINGS SHALL BE ADEQUATELY RESTRAINED WITH CONCRETE BLOCKING PER STANDARD AS LISTED IN THESE PLANS. ALL FITTINGS TO BE BLOCKED MUST BE THOROUGHLY WRAPPED IN PLASTIC SHEETING PRIOR TO PLACING CONCRETE.

**WML 34** ALL GATE VALVES SHALL BE DUCTILE IRON RESILIENT WEDGE, TWO HUNDRED AND FIFTY (250) POUNDS PER SQUARE INCH (PSI), AS MANUFACTURED BY AMERICAN FLOW CONTROL OR APPROVED EQUIVALENT WHICH MEETS OR EXCEEDS THE REQUIREMENTS OF ANSI/AWWA C509.

**WML 35** VALVE BOXES ARE TO BE TYLER 6500 SCREW TYPE FOR MAIN AND WATCH VALVES WITH FORD F-1000 CORPORATION STOPS, AND TYLER 94E CURB BOXES. VALVE BOXES ARE TO BE HEAVY DUTY IF LOCATED IN PAVED AREAS. CONTROL VALVES SHALL BE FORD Z44-333 VALVES.

**WML 36** WATER MAIN CLEANING AND FLUSHING SHALL CONFORM TO ITEM 801.11 CMSC.

**WML 37** HYDROSTATIC TESTS (AS REQUIRED IN SECTION 5 OF AWWA C600) SHALL CONFORM TO CMSC ITEM 801.12. CHLORINATION OF COMPLETED PIPE LINE SHALL CONFORM TO CMSC ITEM 801.13.

**WML 40** ALL WATER LINES SHALL BE TESTED (AWWA 600) AND STERILIZED (AWWA C651) BY THE CONTRACTOR IN ACCORDANCE WITH THE VILLAGE OF ASHVILLE AND AWWA SPECIFICATIONS. TESTING SHALL BE DONE UNDER THE SUPERVISION OF THE VILLAGE ENGINEER OR HIS AUTHORIZED REPRESENTATIVE AT THE CONTRACTOR'S EXPENSE.

**WML 43** THE PRESSURE REQUIRED TO PERFORM HYDROSTATIC TESTING ON WATER LINES MUST BE 150 PSI FOR DOMESTIC LINES AND 200 PSI FOR FIRE SUPPRESSION LINES (PER NFPA 13). HYDROSTATIC TESTS (AS REQUIRED IN SECTION 5 OF AWWA C 600) SHALL CONFORM TO ITEM 801.12 CMSC.

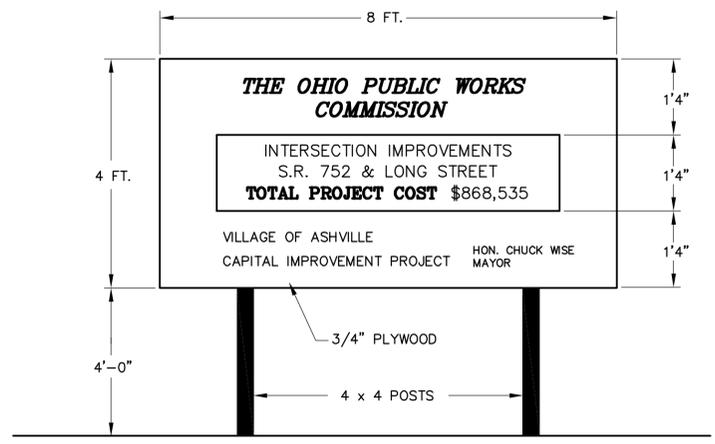
**TEMPORARY SEDIMENT & EROSION CONTROL STANDARDS AND CRITERIA**

IN ORDER TO CONTROL SEDIMENT POLLUTION OF WATER RESOURCES, THE CONTRACTOR SHALL USE CONSERVATION PLANNING AND PRACTICES TO MAINTAIN THE LEVEL OF CONSERVATION ESTABLISHED BY THE FOLLOWING STANDARDS:

1. TIMING OF SEDIMENT-TRAPPING PRACTICES. SEDIMENT CONTROL PRACTICES SHALL BE FUNCTIONAL THROUGHOUT EARTH-DISTURBING ACTIVITIES. PERIMETER CONTROLS, AND OTHER PRACTICES INTENDED TO TRAP SEDIMENT SHALL BE IMPLEMENTED AS THE FIRST STEP OF GRADING AND WITHIN SEVEN DAYS FROM THE START OF GRUBBING. THEY SHALL CONTINUE TO FUNCTION UNTIL THE UPSLOPE DEVELOPMENT AREA IS RESTABILIZED.
2. STABILIZATION OF DENUDED AREA. DENUDED AREAS SHALL HAVE SOIL STABILIZATION APPLIED WITHIN SEVEN DAYS IF THEY ARE TO REMAIN DORMANT FOR MORE THAN FORTY-FIVE DAYS. PERMANENT OR TEMPORARY SOIL STABILIZATION SHALL BE APPLIED TO DENUDED AREAS WITHIN SEVEN DAYS AFTER FINAL GRADE IS REACHED ON ANY PORTION OF THE SITE, AND SHALL ALSO BE APPLIED WITHIN SEVEN DAYS TO DENUDED AREAS WHICH MAY NOT BE AT FINAL GRADE, BUT WILL REMAIN DORMANT (UNDISTURBED) FOR LONGER THAN FORTY-FIVE DAYS.
3. SEDIMENT BARRIER. STREET FLOW RUNOFF FROM DENUDED AREAS SHALL BE FILTERED.
4. STORM SEWER INLET PROTECTION. ALL STORM SEWER INLETS WHICH ACCEPT WATER RUNOFF FROM THE DEVELOPMENT AREA SHALL BE PROTECTED SO THAT SEDIMENT-LADEN WATER WILL NOT ENTER THE STORM SEWER SYSTEM WITHOUT FIRST BEING FILTERED OR OTHER-WISE TREATED TO REMOVE SEDIMENT.
5. CONSTRUCTION ACCESS ROUTES. MEASURES SHALL BE TAKEN TO PREVENT SOIL TRANSPORT ONTO SURFACES WHERE RUNOFF IS NOT CHECKED BY SEDIMENT CONTROLS, OR ONTO PUBLIC ROADS.

**TEMPORARY SEDIMENT & EROSION CONTROL STANDARDS AND CRITERIA (CONTINUED)**

6. CUT AND FILL SLOPES. CUT AND FILL SLOPES SHALL BE CONSTRUCTED IN A MANNER WHICH WILL MINIMIZE EROSION. CONSIDERATION SHALL BE GIVEN TO THE LENGTH AND STEEPNESS OF THE SLOPE, SOIL TYPE, UPSLOPE DRAINAGE AREA, GROUNDWATER CONDITIONS, AND SLOPE STABILIZATION.
7. ESTABLISHMENT OF PERMANENT VEGETATION. A PERMANENT VEGETATION SHALL NOT BE CONSIDERED ESTABLISHED UNTIL GROUND COVER IS ACHIEVED WHICH, IN THE OPINION OF THE VILLAGE ENGINEER, PROVIDES ADEQUATE COVER AND IS MATURE ENOUGH TO CONTROL SOIL EROSION SATISFACTORILY AND TO SURVIVE ADVERSE WEATHER CONDITIONS.
8. DISPOSITION OF TEMPORARY PRACTICES. ALL TEMPORARY EROSION AND SEDIMENT CONTROL PRACTICES SHALL BE DISPOSED OF WITHIN THIRTY DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY PRACTICES ARE NO LONGER NEEDED, UNLESS OTHERWISE AUTHORIZED BY THE APPROVING AGENCY. TRAPPED SEDIMENT SHALL BE PERMANENTLY STABILIZED TO PREVENT FURTHER EROSION.
9. MAINTENANCE. ALL TEMPORARY AND PERMANENT EROSION AND SEDIMENT CONTROL PRACTICES SHALL BE DESIGNED AND CONSTRUCTED TO MINIMIZE MAINTENANCE REQUIREMENTS. THEY SHALL BE MAINTAINED AND REPAIRED AS NEEDED TO ASSURE CONTINUED PERFORMANCE OF THEIR INTENDED FUNCTION.



COLOR SPECIFICATIONS: 10% SCREEN OF PANTONE MATCHING SYSTEM 548 DARK BLUE FOR THE STATE OF OHIO GRAPHIC, PANTONE MATCHING SYSTEM 448 DARK BLUE FOR OHIO PUBLIC WORKS COMMISSION, PROJECT NAME, AND TOTAL PROJECT COST. BLACK WILL BE USED FOR THE BOX AROUND THE PROJECT NAME, TOTAL PROJECT COST, AND LOCAL PROJECT INFORMATION.

**PROJECT SIGN**

LEGEND	
LINE TYPES	
WORK LIMITS	-----
EX. ELECTRIC	-----E-----E-----
EX. FENCE	---X---X---X---X---X---
EX. GAS	-----G-----G-----
EX. RIGHT-OF-WAY	=====
EX. SANITARY SEWER	-----SA-----
EX. WATER	-----W-----W-----
EX. STORM SEWER	-----ST-----
EX. UNDERGROUND TELEPHONE	-----UGT-----UGT-----
EX. UNDERGROUND ELECTRIC	-----UGE-----UGE-----
SYMBOLS	
EX. CATCH BASIN	□
PROP. CATCH BASIN	■
EX. CURB INLET	≡
EX. MANHOLE	○
PROPOSED MANHOLE	●
EX. ELEVATIONS	+891.5
PROP. ELEVATIONS	+931.0
EX. FIRE HYDRANT	⊕
EX. GAS VALVE	gv⊗
EX. SHRUB ROW	⊖
EX. TREE W/SIZE	⊗
EX. WATER VALVE	wvs
EX. IRON PIN	○
EX. SIGN	--- --- ---
PROP. SIGN	--- --- ---
EX. MAILBOX	MB
EX. ELECTRIC PEDESTAL	⊞
EX. TELEPHONE PEDESTAL	⊞
EX. GUY WIRE	—D
BENCHMARK	⊕
EX. LIGHT POLE	⊕
EX. POWER POLE	⊕
ABBREVIATIONS	
EXISTING. . . . .	.EX.
EDGE OF PAVEMENT. . . . .	.E/P
TOP OF CURB. . . . .	.T/C
TOP OF CASTING. . . . .	.TC
ELEVATION. . . . .	.ELEV.
PROPOSED. . . . .	.PROP.
RIGHT-OF-WAY. . . . .	.R/W
HIGH POINT. . . . .	.H.P.

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**ITEM 614: MAINTAINING TRAFFIC, AS PER PLAN**

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE VILLAGE OF ASHVILLE. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT, IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE VILLAGE OF ASHVILLE, SHALL NOT BE PERMITTED.

TRAFFIC CONTROL DEVICES SHALL BE SET UP PRIOR TO THE START OF CONSTRUCTION AND PROPERLY MAINTAINED THROUGHOUT THE PROJECT. THEY SHALL REMAIN IN PLACE ONLY AS LONG AS THEY ARE NEEDED AND SHALL BE IMMEDIATELY REMOVED THEREAFTER. WHERE OPERATIONS ARE PERFORMED IN STAGES, THERE SHALL BE IN PLACE ONLY THOSE DEVICES THAT APPLY TO THE CONDITIONS DURING THAT PARTICULAR STAGE. ALL SIGNS WITH MESSAGES WHICH DO NOT APPLY DURING A CERTAIN PERIOD SHALL BE COVERED OR REMOVED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

THE CONTRACTOR SHALL NOTIFY THE VILLAGE OF ASHVILLE AND ODOT DISTRICT 6 TWO WEEKS PRIOR TO BEGINNING WORK AND WEEKLY THEREAFTER TO DISCUSS ANY CHANGES TO THE MAINTENANCE OF TRAFFIC PLAN AND/OR COMPLETION DATE. BEFORE WORK IS STARTED ON THIS PROJECT THE CONTRACTOR SHALL SUBMIT A WRITTEN SCHEDULE OF OPERATION AND A TRAFFIC MAINTENANCE CONTROL PLAN FOR APPROVAL. NO WORK SHALL BE STARTED THAT WILL RESTRICT ANY LANE USAGE UNLESS IT IS THE INTENT OF THE CONTRACTOR TO WORK FULL TIME WITH A FULL FORCE IN ORDER TO COMPLETE THE WORK WITH NO UNNECESSARY DELAYS.

THE FOLLOWING DEVICES MUST MEET NCHRP 350 BEFORE THE DEVICES ARE INSTALLED ON THE PROJECT: DRUMS, CONES, VERTICAL PANELS AND THE PANEL SUPPORT, PORTABLE SIGN SUPPORTS, TEMPORARY IMPACT ATTENUATORS, TEMPORARY CONCRETE BARRIER, AND BARRICADES.

ALL CONSTRUCTION SIGNAGE SHALL BE INSTALLED AND COVERED BEFORE CONSTRUCTION SHALL BEGIN. AFTER CONSTRUCTION SIGN INSTALLATION, THE CONTRACTOR SHALL NOTIFY THE VILLAGE OF ASHVILLE AND ODOT DISTRICT 6 THREE WORKING DAYS BEFORE WORK BEGINS AND REQUEST AN INSPECTION OF ALL SIGNAGE.

FACES OF CONSTRUCTION SIGNS AND REFLECTIVE SHEETING ON BARRICADES SHALL BE TYPE H (VIP). ALL ORANGE CONSTRUCTION SIGNS SHALL BE FLUORESCENT ORANGE. ALL SHEETING WILL BE TESTED FOR REFLECTIVITY PER ODOT 730.192. VERTICAL PANELS AND DRUM BANDS SHALL BE REFLECTORIZED WITH TYPE G (HIGH INTENSITY) SHEETING COMPLYING WITH THE REQUIREMENTS OF 730.19 ALL SIGNS AND BARRICADES, VERTICAL PANELS, AND DRUMS WILL BE LIKE NEW AND IN GOOD CONDITION IN CONFORMANCE WITH "QUALITY STANDARDS FOR WORK ZONE TRAFFIC CONTROL DEVICES" PUBLISHED BY ATSSA.

MAINTENANCE OF ALL CONTRACTOR SUPPLIED SIGNS, BARRICADES, VERTICAL PANELS AND DRUMS ARE THE CONTRACTOR'S RESPONSIBILITY. IF THE CONTRACTOR FAILS TO CORRECT DEFICIENCIES WITHIN FOUR HOURS OF NOTIFICATION, THE VILLAGE OF ASHVILLE SHALL CORRECT OR HIRE SOMEONE TO CORRECT THE DEFICIENCIES. THE CONTRACTOR SHALL THEN BE BACK-CHARGED PER ODOT SPECIFICATION 614. IN THE CASE THAT BACK-CHARGING THE CONTRACTOR IS NOT APPLICABLE, THE VILLAGE OF ASHVILLE/COUNTY WILL RESCIND AND WITHHOLD ALL PERMITS ISSUED TO THE CONTRACTOR TO WORK WITHIN THE RIGHT-OF-WAY UNTIL THE ISSUE IS SETTLED.

THESE PROVISIONS SHALL NOT IN ANY WAY RELIEVE THE CONTRACTOR OF ANY OF HIS LEGAL RESPONSIBILITIES OR LIABILITIES FOR THE SAFETY OF THE PUBLIC.

ALL BARRICADES AT CLOSURES SHALL HAVE YELLOW TYPE A LOW INTENSITY FLASHING WARNING LIGHTS. ALL BARRELS SHALL HAVE YELLOW TYPE C STEADY BURN LIGHTS. ALL SIGNS NINE SQUARE FEET (36" X 36") AND OVER SHALL HAVE YELLOW TYPE A LOW INTENSITY FLASHING WARNING LIGHTS AND TWO FLAGS.

**ITEM 614: MAINTAINING TRAFFIC, AS PER PLAN (CONTINUED)**

DRUMS SHALL BE PLACED AS FOLLOWS: 40' C/C ON TANGENTS, 10' C/C ON TAPERS; AND 10' C/C IN RADII.

ENOUGH TYPE III BARRICADES SHALL BE PROVIDED TO CLOSE ALL LANES AS DIRECTED BY THE ENGINEER.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH ODOT CMS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE LATEST VERSION OF THE OMUTCD. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, AS PER PLAN, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

PERMANENT STRIPING OR CLASS I TEMPORARY STRIPING SHALL BE INSTALLED NO LATER THAN FOURTEEN (14) CALENDAR DAYS AFTER THE FINISH PAVING COURSE IS COMPLETED. THE PAVING CONTRACTOR SHALL BE RESPONSIBLE TO NOTIFY THE STRIPING CONTRACTOR TO INSURE THE PERMANENT STRIPING IS INSTALLED WITHIN THE FOURTEEN (14) CALENDAR DAY LIMIT. ALL PRE-MARKING MATERIALS AND ASSOCIATED COSTS SHALL BE INCLUDED WITHIN ITEM 614-MAINTAINING TRAFFIC AS PER PLAN.

AT LEAST ONE LANE IN EACH DIRECTION ON STATE ROUTE 752 SHALL BE MAINTAINED AT ALL TIMES EXCEPT DURING ALLOWABLE WORKING HOURS.

DROPOFF CONDITIONS SHALL MEET STANDARD DRAWING MT-101.90 OUTSIDE OF WORKING HOURS.

**PERMANENT TRAFFIC CONTROL**

THE ROADWAY SHALL NOT BE OPENED TO TRAFFIC UNTIL PERMANENT TRAFFIC CONTROLS ARE IN PLACE OR UNTIL TEMPORARY TRAFFIC CONTROLS, APPROVED BY THE VILLAGE ENGINEER, ARE INSTALLED. THE CONTRACTOR ASSUMES ALL LIABILITY FOR THE PREMATURE REMOVAL OF TEMPORARY TRAFFIC CONTROL.

ALL PERMANENT TRAFFIC CONTROLS NOT IN CONFLICT WITH THE TEMPORARY TRAFFIC CONTROLS SHALL BE MAINTAINED THROUGHOUT THE PROJECT BY THE CONTRACTOR. THE CONTRACTOR SHALL ASSUME ALL LIABILITY FOR MISSING, DAMAGED, AND IMPROPERLY PLACED TRAFFIC CONTROL DEVICES.

**ITEM 614 - LAW ENFORCEMENT OFFICER (WITH PATROL CAR)**

IN ADDITION TO THE REQUIREMENTS OF ODOT CMS 614 AND THE OMUTCD, A UNIFORMED LAW ENFORCEMENT OFFICER (LEO) AND OFFICIAL PATROL CAR WITH WORKING TOP MOUNTED EMERGENCY FLASHING LIGHTS, SHALL BE PROVIDED FOR CONTROLLING TRAFFIC FOR THE FOLLOWING TASKS:

1. FOR ALL LANE CLOSURES. LEO'S SHALL BE USED WHERE THE OMUTCD INTENDS THAT FLAGGERS BE USED.
2. DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE WHERE COMPLETE BLOCKAGE OF TRAFFIC IS REQUIRED.
3. DURING A TRAFFIC SIGNAL INSTALLATION.
4. FLAGGING THROUGH THE TRAFFIC SIGNAL AT THE INTERSECTION OF STATE ROUTE 752 AND LONG STREET.

THE LEOS ARE CONSIDERED TO BE EMPLOYED BY THE CONTRACTOR AND THE CONTRACTOR SHALL BE RESPONSIBLE FOR THEIR ACTIONS. ALTHOUGH THEY ARE EMPLOYED BY THE CONTRACTOR, THE ENGINEER SHALL HAVE CONTROL OF THEIR PLACEMENT. THE OFFICIAL PATROL CAR SHALL BE A PUBLIC SAFETY VEHICLE AS REQUIRED BY THE OHIO REVISED CODE. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR THESE SERVICES WITH THE VILLAGE OF ASHVILLE POLICE DEPARTMENT.

LAW ENFORCEMENT OFFICERS (WITH PATROL CAR) REQUIRED BY THE MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR).

THE HOURS PAID SHALL INCLUDE MINIMUM SHOW UP TIME OF 24 HOURS.

**DUST CONTROL**

THE CONTRACTOR SHALL FURNISH AND APPLY WATER-ITEM 616 AND CALCIUM CHLORIDE-ITEM 616, FOR DUST CONTROL AS DIRECTED BY THE ENGINEER. ALL COSTS ARE TO BE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 614 MAINTAINING TRAFFIC.

**LOCAL ACCESS**

INGRESS AND EGRESS SHALL BE MAINTAINED TO ALL RESIDENTIAL AND COMMERCIAL PROPERTIES. DRIVEWAY CLOSURE MAY BE NECESSARY TO ENABLE WORK ON OR IN FRONT OF A DRIVE. THE CONTRACTOR WILL BE RESPONSIBLE FOR NOTIFYING OWNERS, RESIDENTS, OR BUSINESS OPERATORS IN WRITING AT LEAST 48 HOURS BUT NOT MORE THAN 72 HOURS PRIOR TO CLOSURE. THE ENGINEER SHALL BE GIVEN A LIST OF THE PERSONS THAT WERE GIVEN NOTICES WITH THE DATE OF NOTICE INCLUDED. CLOSURE IS PERMITTED ONLY DURING WORK HOURS AND ACCESS MUST BE RETURNED AT THE END OF EACH WORKING DAY. PROPERTIES WITH MULTIPLE DRIVES MAY HAVE ONE DRIVE CLOSED AT A TIME, WHILE WORK IS PERFORMED IN THE AREA OF THE CLOSED DRIVE.

INDIVIDUAL DRIVE CLOSURES SHALL BE KEPT TO THE MINIMUM TIME NEEDED FOR CONSTRUCTION ACTIVITIES, EVERY EFFORT MUST BE MADE TO ACCOMMODATE THE OWNER'S NEED FOR ACCESS.

ACCESS FOR MAIL AND EMERGENCY SERVICES SHALL BE MAINTAINED AT ALL TIMES.

**PEDESTRIAN ACCESS**

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION AND SAFE MOVEMENT OF PEDESTRIANS THROUGH, AROUND, AND AWAY FROM THE CONSTRUCTION SITE.

THE SAFETY OF PEDESTRIAN TRAFFIC SHALL BE CONSIDERED AT ALL TIMES IN THE PROVISION OF TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS AND NOTES. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PROVIDE LIGHT, SIGNS, BARRICADES, AND OTHER WARNINGS TO PHYSICALLY SEPARATE THE PEDESTRIAN FROM HAZARDS INCIDENTAL TO THE CONSTRUCTION OPERATIONS SUCH AS OPEN EXCAVATIONS, ETC. AT ALL TIMES, THE PEDESTRIAN MOT SHALL BE SUBJECT TO THE APPROVAL OF THE VILLAGE ENGINEER.

**TRENCH FOR WIDENING**

THE OPEN TRENCH SHALL BE ADEQUATELY MAINTAINED AND PROTECTED WITH DRUMS OR BARRICADES AT ALL TIMES. PLACEMENT OF PROPOSED SUBBASE AND BASE MATERIAL SHALL FOLLOW AS CLOSELY AS POSSIBLE BEHIND EXCAVATION OPERATIONS. THE LENGTH OF THE WIDENING TRENCH SHALL OF ALL TIMES BE KEPT TO A MINIMUM AND SUBJECT TO APPROVAL OF THE VILLAGE ENGINEER. ANY OPEN EXCAVATION NOT PROTECTED BY PCB SHALL HAVE A TEMPORARY WEDGE TREATMENT CONSTRUCTED OF FIRM AND UNYIELDING MATERIAL PROVIDING A MAXIMUM 3:1 SLOPE FROM THE TOP OF THE TRENCH TO THE BOTTOM SUBJECT TO THE APPROVAL OF THE VILLAGE ENGINEER.

**OVERNIGHT TRENCH CLOSING**

THE TEMPORARY PAVEMENT AND/OR BASE WIDENING SHALL BE COMPLETED TO A DEPTH OF NO MORE THAN 5 INCHES BELOW THE EXISTING PAVEMENT BY THE END OF EACH WORK DAY. NO TRENCH SHALL BE LEFT OPEN OVERNIGHT. IN CASE WORK MUST BE SUSPENDED BECAUSE OF INCLEMENT WEATHER OR OTHER REASONS, THE TRENCH FOR THE UNCOMPLETED TEMPORARY PAVEMENT AND/OR BASE WIDENING SHALL BE BACKFILLED AT THE DIRECTION OF THE ENGINEER.

**TRAFFIC SIGNAL**

CONTRACTOR SHALL COORDINATE ALL PHASES OF CONSTRUCTION WITH THE VILLAGE OF ASHVILLE. THE CONTRACTOR SHALL NOTIFY THE VILLAGE OF ASHVILLE TO HAVE THE TRAFFIC SIGNAL PUT ON YELLOW/RED FLASHING DURING CONSTRUCTION, PER ALL O.D.O.T. & O.M.U.T.C.D. REQUIREMENTS.

**WORKING HOURS**

DAY OF THE WEEK	WORKING TIME
SUNDAY	NONE
MONDAY - FRIDAY	8 A.M. TO 4 P.M.
SATURDAY	9 A.M. TO 4 P.M.

**HOLIDAY'S**

NO WORK SHALL BE PERFORMED AND ALL EXISTING LANES SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR EVENTS:

CHRISTMAS	NEW YEARS
MEMORIAL DAY	THANKSGIVING
LABOR DAY	JULY 4TH

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

DAY OF THE WEEK	TIME ALL LANES MUST BE OPEN TO TRAFFIC
SUNDAY	12:00N FRIDAY THROUGH 12:00N MONDAY
MONDAY	12:00N FRIDAY THROUGH 12:00N TUESDAY
TUESDAY	12:00N MONDAY THROUGH 12:00N WEDNESDAY
WEDNESDAY	12:00N TUESDAY THROUGH 12:00N THURSDAY
THURSDAY	12:00N WEDNESDAY THROUGH 12:00N MONDAY
FRIDAY	12:00N THURSDAY THROUGH 12:00N MONDAY
SATURDAY	12:00N FRIDAY THROUGH 12:00N MONDAY

WITHIN THE LENGTH OF CONSTRUCTION, PROVISIONS SHALL BE MADE TO CONTROL TRAFFIC ENTERING FROM INTERSECTING STREETS AND MAJOR DRIVES AS NECESSARY TO PREVENT WRONG WAY MOVEMENTS AND TO KEEP VEHICLES OFF OF NEW PAVEMENT.

**SEQUENCE OF CONSTRUCTION**

PHASES 1 THRU 6 SHALL INCLUDE BUT NOT BE LIMITED TO ALL PAVEMENT, BERM, DRIVE APPROACHES, SIDE STREETS, CURBS, CURB RAMPS, STORM SEWER, STORM SEWER STRUCTURES AND TRAFFIC POLES. PHASES 1 THRU 6 SHALL NOT INCLUDE ITEM 448 - 1-1/4" ASPHALT CONCRETE SURFACE COURSE AND THE PERMANENT PAVEMENT MARKINGS AND SIGNAGE.

THE CONTRACTOR SHALL COMPLETE EACH PHASE AND OBTAIN THE APPROVAL OF THE VILLAGE ENGINEER PRIOR TO BEGINNING THE NEXT PHASE OF THE CONSTRUCTION.

**PHASE 1**

PHASE 1 CONSTRUCTION WILL INVOLVE THE SOUTH SIDE OF THE WEST LEG OF STATE ROUTE 752 FROM STA. 90+60 TO STA. 94+77. THIS PHASE OF CONSTRUCTION WILL UTILIZE A FLAGGER CLOSING 1-LANE OF A 2-LANE ROAD, PER ODOT STANDARD CONSTRUCTION DRAWING - MT 97.10.

**PHASE 2**

PHASE 2 CONSTRUCTION WILL INVOLVE THE SOUTH SIDE OF THE EAST LEG OF STATE ROUTE 752 FROM STA. 95+13 TO STA. 99+29. THIS PHASE OF CONSTRUCTION WILL UTILIZE A FLAGGER CLOSING 1-LANE OF A 2-LANE ROAD, PER ODOT STANDARD CONSTRUCTION DRAWING - MT 97.10.

**PHASE 3**

PHASE 3 CONSTRUCTION WILL INVOLVE THE NORTH SIDE OF STATE ROUTE 752 FROM STA. 90+60 TO STA. 99+29. THIS PHASE OF CONSTRUCTION WILL UTILIZE A LANE SHIFT AND MAINTAIN 2-WAY/2-LANES OF TRAFFIC.

**PHASE 4**

PHASE 4 CONSTRUCTION WILL INVOLVE THE WEST SIDE OF THE SOUTH LEG OF LONG STREET FROM STA. 6+53 TO 9+87. THIS PHASE OF CONSTRUCTION WILL UTILIZE A FLAGGER CLOSING 1-LANE OF A 2-LANE ROAD, PER ODOT STANDARD CONSTRUCTION DRAWING - MT 97.10.

**PHASE 5**

PHASE 5 CONSTRUCTION WILL INVOLVE THE WEST SIDE OF THE NORTH LEG OF LONG STREET FROM STA. 10+10 TO STA. 13+68. THIS PHASE OF CONSTRUCTION WILL UTILIZE A FLAGGER CLOSING 1-LANE OF A 2-LANE ROAD, PER ODOT STANDARD CONSTRUCTION DRAWING - MT 97.10.

**PHASE 6**

PHASE 6 CONSTRUCTION WILL INVOLVE THE EAST SIDE OF LONG STREET FROM STA. 6+53 TO STA. 13+68. THIS PHASE OF CONSTRUCTION WILL UTILIZE A LANE SHIFT AND MAINTAIN 2-WAY/2-LANES OF TRAFFIC.

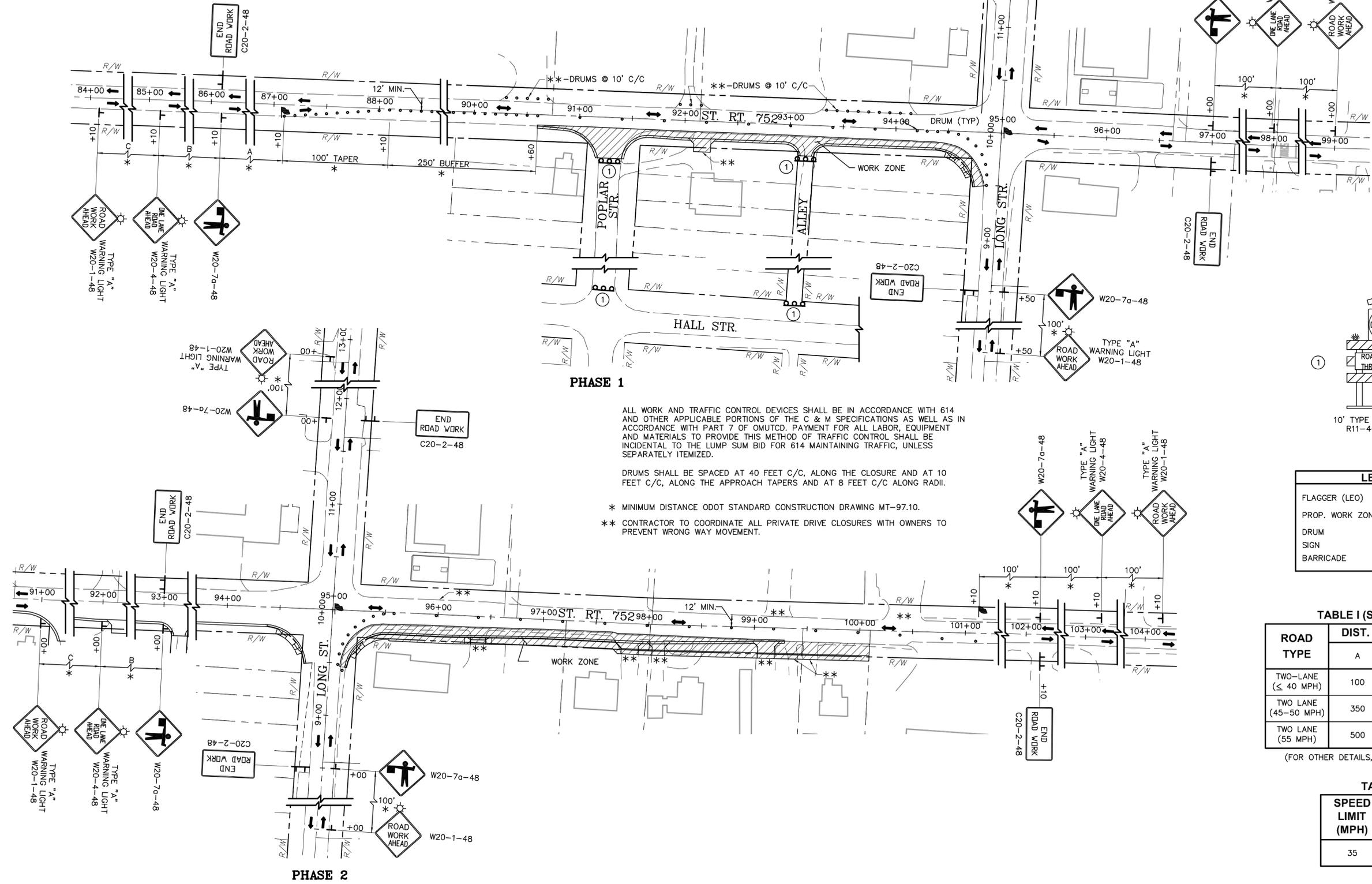
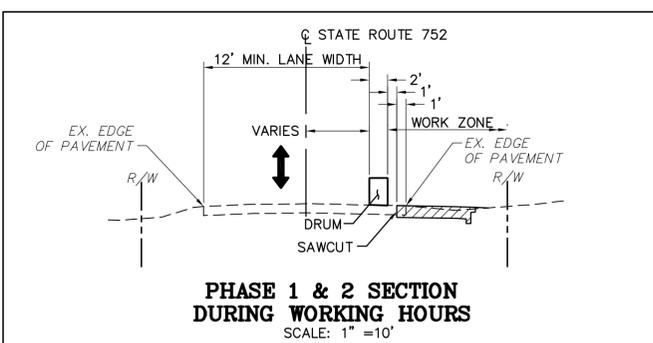
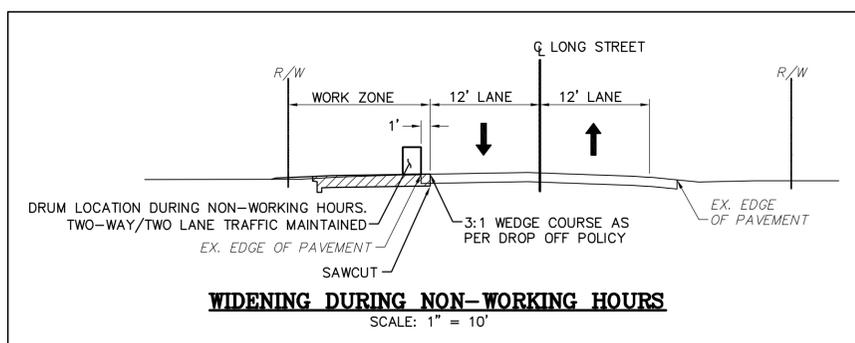


CALCULATED WRA CHECKED BJS

**MAINTENANCE OF TRAFFIC - PHASE 1 & 2**  
**STATE ROUTE 752 AND LONG STREET**

**PIC-752/LONG ST.**

10  
37

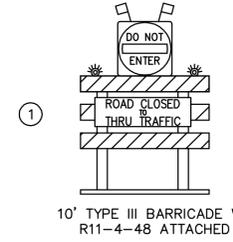


**PHASE 1**

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH 614 AND OTHER APPLICABLE PORTIONS OF THE C & M SPECIFICATIONS AS WELL AS IN ACCORDANCE WITH PART 7 OF ODOT. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS TO PROVIDE THIS METHOD OF TRAFFIC CONTROL SHALL BE INCIDENTAL TO THE LUMP SUM BID FOR 614 MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED.

DRUMS SHALL BE SPACED AT 40 FEET C/C, ALONG THE CLOSURE AND AT 10 FEET C/C, ALONG THE APPROACH TAPERS AND AT 8 FEET C/C ALONG RADII.

- \* MINIMUM DISTANCE ODOT STANDARD CONSTRUCTION DRAWING MT-97.10.
- \*\* CONTRACTOR TO COORDINATE ALL PRIVATE DRIVE CLOSURES WITH OWNERS TO PREVENT WRONG WAY MOVEMENT.



**LEGEND**

FLAGGER (LEO)	
PROP. WORK ZONE	
DRUM	
SIGN	
BARRICADE	

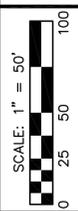
**TABLE I (SIGN SPACING)**

ROAD TYPE	DIST. BETWEEN SIGNS (FT)		
	A	B	C
TWO-LANE (≤ 40 MPH)	100	100	100
TWO LANE (45-50 MPH)	350	350	350
TWO LANE (55 MPH)	500	500	500

(FOR OTHER DETAILS, SEE ODOT SCD MT-97.10)

**TABLE II**

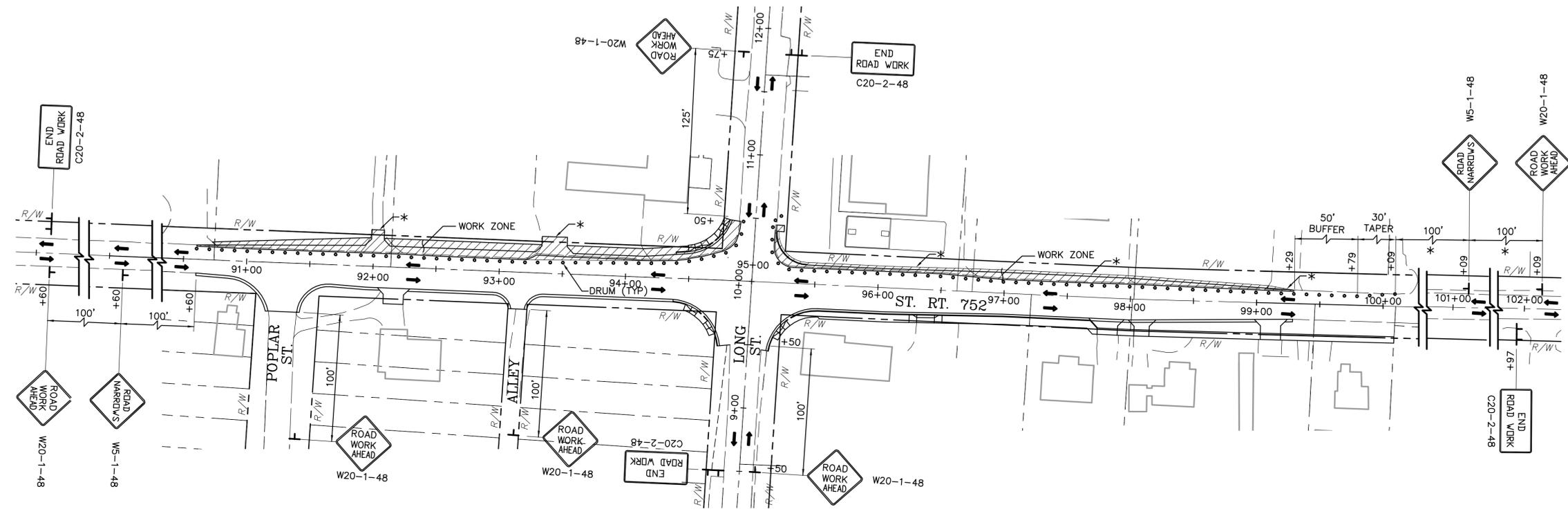
SPEED LIMIT (MPH)	BUFFER (D) FT MINIMUM
35	250



CALCULATED  
WRA  
CHECKED  
BJS

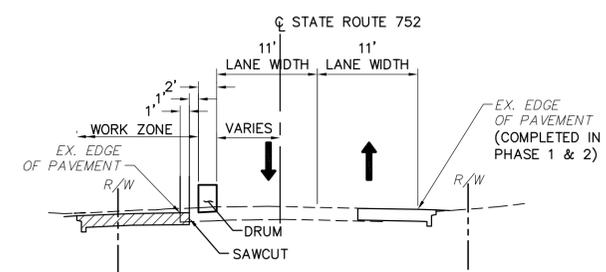
**MAINTENANCE OF TRAFFIC - PHASE 3  
STATE ROUTE 752 AND LONG STREET**

**PIC-752/LONG ST.**



**PHASE 3**

LEGEND	
FLAGGER (LEO)	
PROP. WORK ZONE	
DRUM	
SIGN	
BARRICADE	



**PHASE 3 SECTION  
DURING WORKING HOURS**  
SCALE: 1" = 10'

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH 614 AND OTHER APPLICABLE PORTIONS OF THE C & M SPECIFICATIONS AS WELL AS IN ACCORDANCE WITH PART 7 OF OMUTCD. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS TO PROVIDE THIS METHOD OF TRAFFIC CONTROL SHALL BE INCIDENTAL TO THE LUMP SUM BID FOR 614 MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED.

DRUMS SHALL BE SPACED AT 40 FEET C/C, ALONG THE CLOSURE AND AT 10 FEET C/C, ALONG THE APPROACH TAPERS AND AT 8 FEET C/C ALONG RADII.

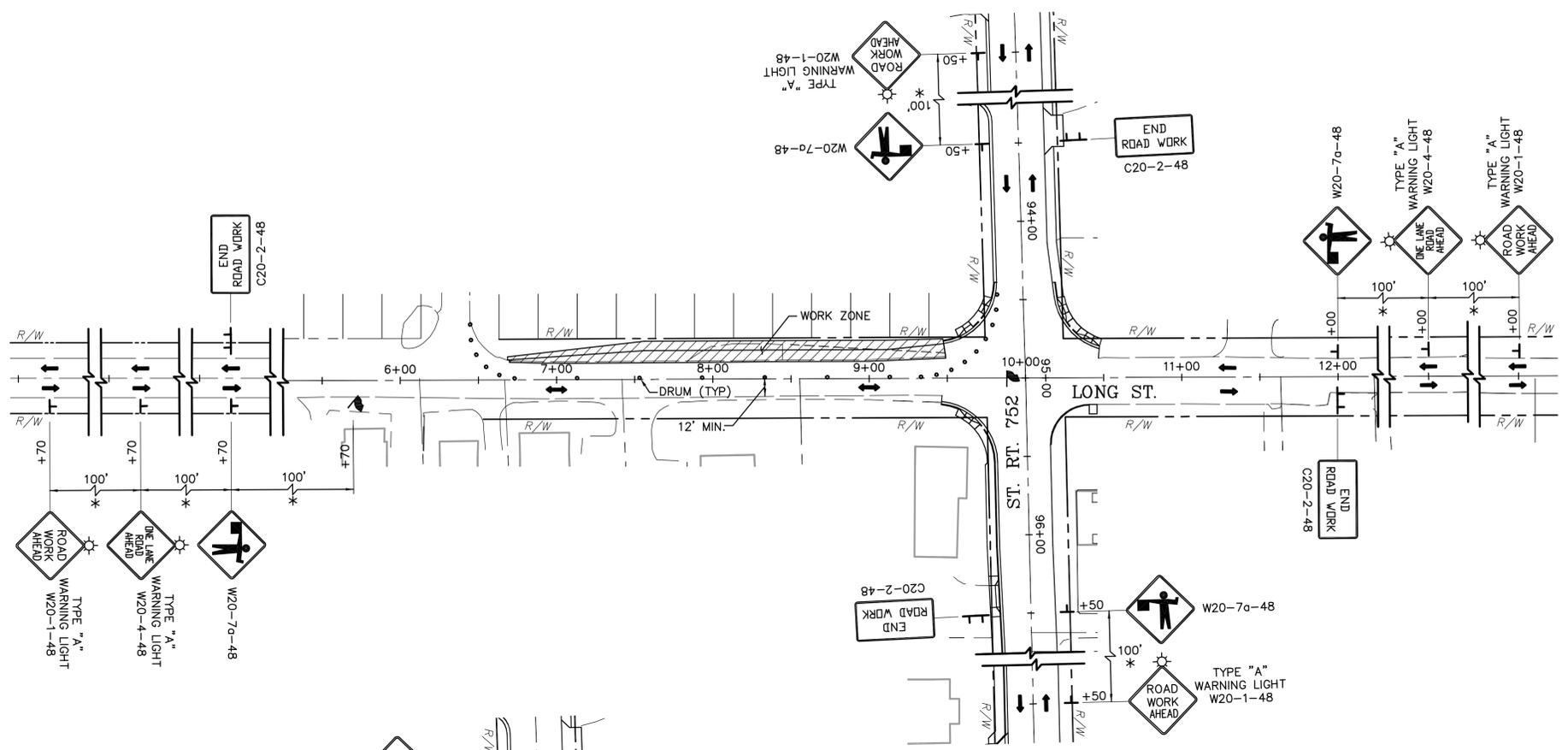
- \* MINIMUM DISTANCE ODOT STANDARD CONSTRUCTION DRAWING MT-97.10.
- \*\* CONTRACTOR TO COORDINATE ALL PRIVATE DRIVE CLOSURES WITH OWNERS TO PREVENT WRONG WAY MOVEMENT.



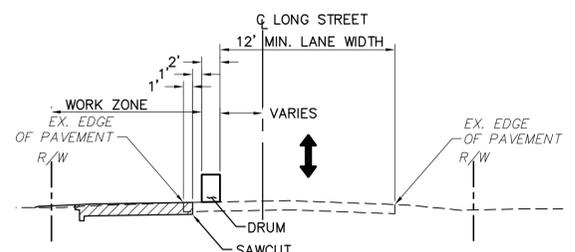
CALCULATED WRA BJS  
CHECKED BJS

**MAINTENANCE OF TRAFFIC - PHASE 4 & 5  
STATE ROUTE 752 AND LONG STREET**

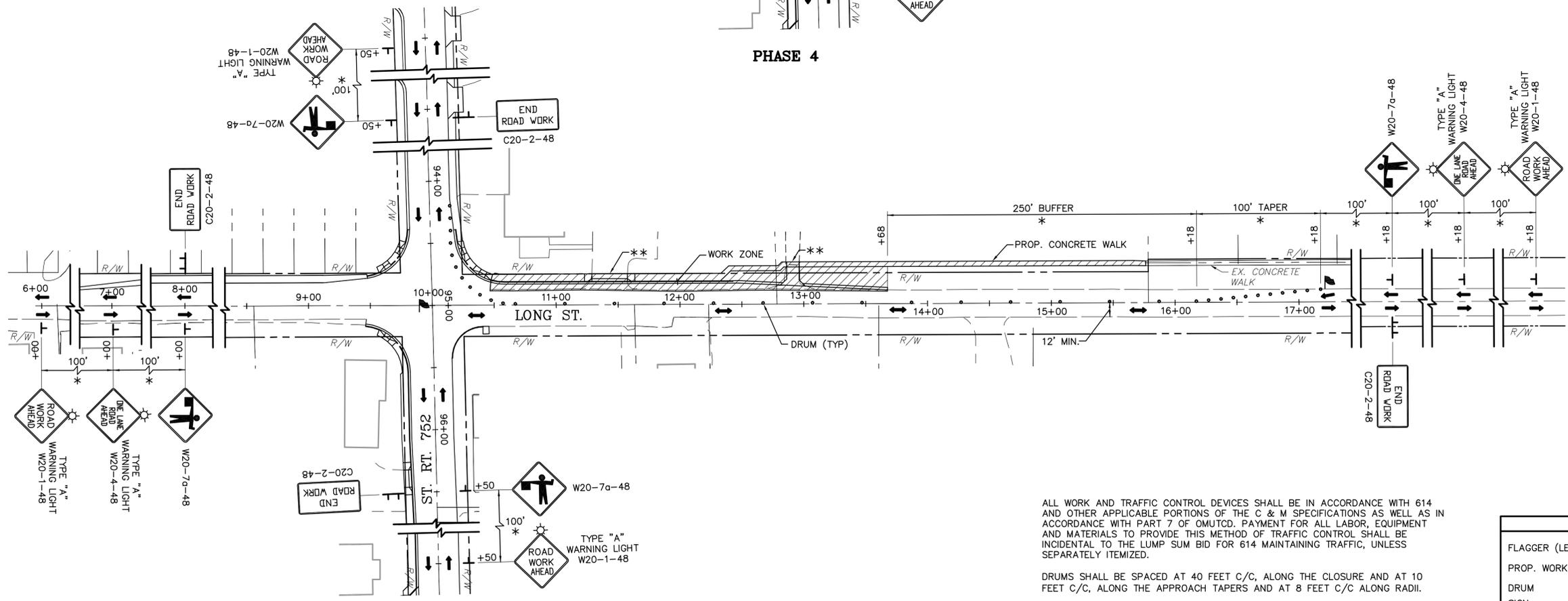
**PIC-752/LONG ST.**



**PHASE 4**



**PHASE 4 & 5 SECTION  
DURING WORKING HOURS**  
SCALE: 1" = 10'



**PHASE 5**

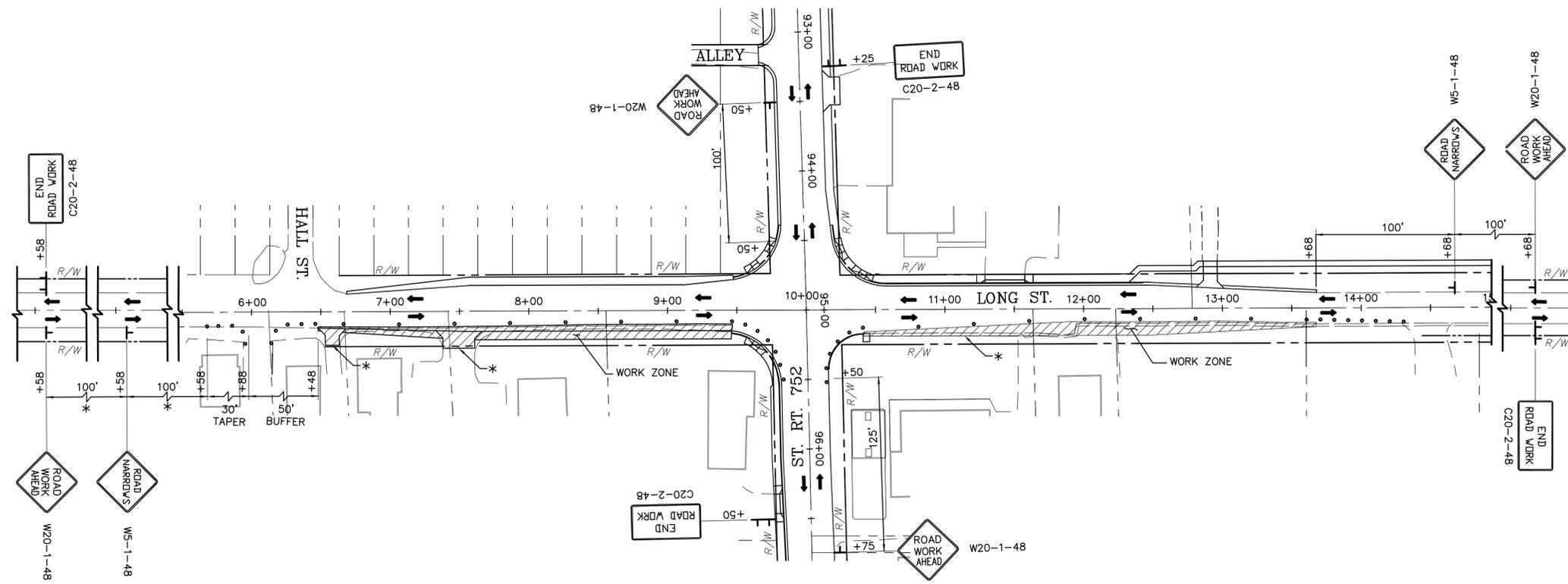
ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH 614 AND OTHER APPLICABLE PORTIONS OF THE C & M SPECIFICATIONS AS WELL AS IN ACCORDANCE WITH PART 7 OF OMTCD. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS TO PROVIDE THIS METHOD OF TRAFFIC CONTROL SHALL BE INCIDENTAL TO THE LUMP SUM BID FOR 614 MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED.

DRUMS SHALL BE SPACED AT 40 FEET C/C, ALONG THE CLOSURE AND AT 10 FEET C/C, ALONG THE APPROACH TAPERS AND AT 8 FEET C/C ALONG RADII.

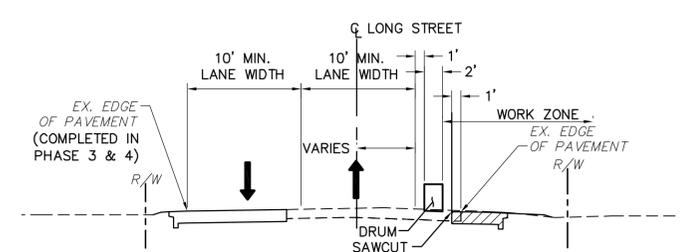
\* MINIMUM DISTANCE ODOT STANDARD CONSTRUCTION DRAWING MT-97.10.

\*\* CONTRACTOR TO COORDINATE ALL PRIVATE DRIVE CLOSURES WITH OWNERS TO PREVENT WRONG WAY MOVEMENT.

LEGEND	
FLAGGER (LEO)	
PROP. WORK ZONE	
DRUM	
SIGN	
BARRICADE	



**PHASE 6**



**PHASE 6 SECTION  
DURING WORKING HOURS**  
SCALE: 1" = 10'

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH 614 AND OTHER APPLICABLE PORTIONS OF THE C & M SPECIFICATIONS AS WELL AS IN ACCORDANCE WITH PART 7 OF ODOTCD. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS TO PROVIDE THIS METHOD OF TRAFFIC CONTROL SHALL BE INCIDENTAL TO THE LUMP SUM BID FOR 614 MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED.

DRUMS SHALL BE SPACED AT 40 FEET C/C, ALONG THE CLOSURE AND AT 10 FEET C/C, ALONG THE APPROACH TAPERS AND AT 8 FEET C/C ALONG RADII.

- \* MINIMUM DISTANCE ODOT STANDARD CONSTRUCTION DRAWING MT-97.10.
- \*\* CONTRACTOR TO COORDINATE ALL PRIVATE DRIVE CLOSURES WITH OWNERS TO PREVENT WRONG WAY MOVEMENT.

LEGEND	
FLAGGER (LEO)	
PROP. WORK ZONE	
DRUM	
SIGN	
BARRICADE	

CALCULATED WRA CHECKED BJS

SCALE: 1" = 50'

**MAINTENANCE OF TRAFFIC - PHASE 6  
STATE ROUTE 752 AND LONG STREET**

**PIC-752/LONG ST.**

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SHEET NUMBER										
ITEM #	8	9	36	37	ITEM #	TOTAL	UNIT	DESCRIPTION		
<b>TRAFFIC CONTROL</b>										
630			102.19	89.88	630	192.07	SF	SIGN, FLAT SHEET		
630			136	151	630	287	LF	GROUND MOUNTED SUPPORT, NO. 3 POST		
630			67	13	630	80	LF	GROUND MOUNTED SUPPORT, NO. 4 POST		
630				2	630	2	EA	REMOVAL OF GROUND MOUNTED SIGN & STORAGE, AS PER PLAN		
644			370	263	644	633	LF	CHANNELIZING LINE, 8" WHITE		
644			0.23	0.18	644	0.41	MI	CENTER LINE, 4" DOUBLE YELLOW		
644			0.30	0.24	644	0.54	MI	EDGE LINE, 4" WHITE		
644			90	182	644	356	LF	CROSSWALK LINE, 10" WHITE		
644			4	4	644	8	EA	LANE ARROW, WHITE		
644			62	46	644	108	LF	STOP LINE, 20" WHITE		
644			145	139	644	284	LF	TRANSVERSE LINE, 24" YELLOW		
644			2	2	644	4	EA	WORD ON PAVEMENT, 96" ("ONLY" WHITE)		
644			1		644	1	EA	WORD ON PAVEMENT, 120" ("SCHOOL" WHITE)		
644			22		644	22	LF	SCHOOL ZONE LINE, 16" WHITE		
<b>MAINTAINING TRAFFIC</b>										
614					614	LUMP	SUM	MAINTAINING TRAFFIC, AS PER PLAN		
614		24			614	24	HR	LAW ENFORCEMENT OFFICER, WITH PATROL CAR		
<b>MISCELLANEOUS</b>										
619					619	6	MONTH	FIELD OFFICE, TYPE A		
623					623	LUMP	SUM	CONSTRUCTION LAYOUT STAKES		
624					624	LUMP	SUM	MOBILIZATION		
SPEC	2				SPEC	2	EA	PROJECT SIGN		

ALL ITEMS ARE ODOT UNLESS OTHERWISE SPECIFIED AS CITY OF COLUMBUS (COC)

SHEET NUMBER															
ITEM #	5&6	15	16	17	18	19	20	26	32	34	35	ITEM #	TOTAL	UNIT	DESCRIPTION
<b>ROADWAY</b>															
201												201	LUMP	SUM	CLEARING AND GRUBBING
201				1								201	1	EA	TREE REMOVED, 20" SIZE
201												201	1	EA	TREE REMOVED, 36" SIZE
202			59				98					202	157	FT	8" PIPE REMOVED
202			181				183					202	364	FT	8" PIPE ABANDONED IN PLACE
202			13								30	202	43	FT	12" PIPE REMOVED
202												202	74	FT	12" PIPE ABANDONED IN PLACE
202		51	212	54	64	59	32					202	472	SY	PAVEMENT REMOVED
202			2				1					202	3	EA	STRUCTURE REMOVED, CATCH BASIN
202			1									202	1	EA	STRUCTURE REMOVED, HEADWALL
203									537	419		203	956	CY	EXCAVATION EXCLUDING EMBANKMENT CONSTRUCTION
203												203	377	CY	EMBANKMENT
204	8											204	8	HR	PROOF ROLLING
204		374	793	465	428	421	187					204	2668	SY	SUBGRADE COMPACTION
252		417	949	458	762	663	306					252	3555	FT	FULL DEPTH PAVEMENT SAWING
254		661	1372	532	605	707	265					254	4137	SY	PAVEMENT PLANING, ASPHALT CONCRETE
604			1									604	1	EA	REFERENCE MONUMENTS
SPEC		1	1	3								SPEC	5	EA	SPECIAL - MAILBOX REMOVED & RESET
SPEC		1	1	3								SPEC	5	EA	SPECIAL - MAILBOX SUPPORT SYSTEM, SINGLE
SPEC	20											SPEC	20	CY	CONTAMINATED SOIL, REMOVED AND DISPOSED OF
<b>PAVEMENT</b>															
301		44	94	44	56	60	19					301	317	CY	6" BITUMINOUS AGGREGATE BASE (COC)
304	20	55	124	72	66	64	28					304	429	CY	6" AGGREGATE BASE
304		19	12	14	20	14	9					304	88	CY	9" AGGREGATE BASE (BERM) (COC)
407		66	138	54	61	71	26					407	416	GAL	TACK COAT
408		106	224	105	135	144	47					408	761	GAL	BITUMINOUS PRIME COAT
408		42	28	30	41	29	19					408	189	GAL	BITUMINOUS PRIME COAT (BERM)
423		417	949	458	762	663	306					423	3555	LF	CRACK SEALING, TYPE II
448		31	66	27	31	34	13					448	202	CY	1-1/4" ASPHALT CONCRETE SURFACE COURSE, TYPE 2 PG64-22
448		11	25	12	14	13	5					448	80	CY	1-3/4" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2 PG64-22
452											193	452	193	SY	6" NON-REINFORCED CONCRETE PAVEMENT
452			29									452	164	SY	8" NON-REINFORCED CONCRETE PAVEMENT
608			1555	1414		727	1269					608	4965	SF	CONCRETE WALK (COC-STD DWG 2300)
608			6									608	6	EA	CURB RAMPS
609			253			44						609	297	FT	COMBINATION CURB AND GUTTER (COC-STD DWG 2010)
609			40									609	40	FT	STRAIGHT 18" CONCRETE CURB (COC-STD DWG 2000)
<b>DRAINAGE</b>															
601	2											601	2	CY	ROCK CHANNEL PROTECTION, TYPE B WITH FABRIC FILTER
603	20											603	20	FT	4" CONDUIT, TYPE F
603	20											603	20	FT	6" CONDUIT, TYPE F
603	20											603	20	FT	8" CONDUIT, TYPE B
603	20											603	20	FT	12" CONDUIT, TYPE E
604	2											604	2	EA	PRECAST REINFORCED CONCRETE OUTLET
604											1	604	1	EA	MANHOLE, CLASS B, TYPE AA-S100 (COC)
604											3	604	3	EA	CURB & GUTTER INLET, TYPE AA-S125 (COC)
604											15	604	15	EA	CATCH BASIN, TYPE AA-S133 (COC)
604											1	604	1	EA	CATCH BASIN, TYPE AA-S134 WITH 24" SIDE INLETS (COC)
604											2	604	2	EA	ENDWALL FOR 15" PIPE, TYPE AA-S169 (COC)
604											2	604	2	EA	EXISTING CATCH BASIN, ADJUSTED TO GRADE WITH H.D. FRAME AND GRATE (AS PER PLAN)
604		1	1	1	1		1					604	5	EA	MANHOLE ADJUSTED TO GRADE
604			1									604	1	EA	CATCH BASIN, ODOT CB-4.2
605	20											605	20	LF	8" UNCLASSIFIED PIPE UNDERDRAIN
605		416	792	438	486	498	236					605	2866	LF	4" SHALLOW PIPE UNDERDRAIN
901											230	901	230	LF	12" STORM SEWER PIPE, W/TYPE 1 BEDDING W/ITEM 911 COMPACTED BACKFILL (COC)
901											200	901	200	LF	12" STORM SEWER PIPE, W/TYPE 1 BEDDING W/ITEM 912 COMPACTED GRANULAR BACKFILL (COC)
901											163	901	163	LF	15" STORM SEWER PIPE, W/TYPE 1 BEDDING W/ITEM 911 COMPACTED BACKFILL (COC)
901											77	901	77	LF	15" STORM SEWER PIPE, W/TYPE 1 BEDDING W/ITEM 912 COMPACTED GRANULAR BACKFILL (COC)
901											121	901	121	LF	18" STORM SEWER PIPE, W/TYPE 1 BEDDING W/ITEM 911 COMPACTED BACKFILL (COC)
901											20	901	20	LF	18" STORM SEWER PIPE, W/TYPE 1 BEDDING W/ITEM 912 COMPACTED GRANULAR BACKFILL (COC)
901											23	901	23	LF	12" STORM SEWER PIPE, W/WATER TIGHT JOINTS, TYPE 1 BEDDING & ITEM 911 COMPACTED BACKFILL (COC)
901											104	901	104	LF	12" STORM SEWER PIPE, W/WATER TIGHT JOINTS, TYPE 1 BEDDING & ITEM 912 COMPACTED GRANULAR BACKFILL (COC)
901											16	901	16	LF	12" PVC C-900 PIPE, W/TYPE 1 BEDDING & ITEM 911 COMPACTED BACKFILL (COC)
901											32	901	32	LF	12" PVC C-900 PIPE, W/TYPE 1 BEDDING & ITEM 912 COMPACTED GRANULAR BACKFILL (COC)
901											144	901	144	LF	15" STORM SEWER PIPE, W/WATER TIGHT JOINTS, TYPE 1 BEDDING & ITEM 912 COMPACTED GRANULAR BACKFILL (COC)
901											41	901	41	LF	18" STORM SEWER PIPE, W/WATER TIGHT JOINTS, TYPE 1 BEDDING & ITEM 912 COMPACTED GRANULAR BACKFILL (COC)
<b>EROSION CONTROL</b>															
659	309											659	309	CY	TOPSOIL
659	1.39											659	1.39	SY	INTERSEEDING
659	6.3											659	6.3	M SF	MOWING
659	1.20											659	1.20	TON	AGRICULTURAL LIMING
659	0.39											659	0.39	TON	COMMERCIAL FERTILIZER
659								1331	1446			659	2777	SY	SEEDING AND MULCHING
659	15.75											659	15.75	M GAL	WATER
832												832	LUMP	SUM	STORMWATER POLLUTION PREVENTION PLAN, AS PER PLAN (SHT 5/37)
832	20000											832	20000	EA	EROSION CONTROL
<b>WATER</b>															
638			1	2		1						638	4	EA	SPECIAL-VALVE BOX, ADJUSTED TO GRADE (COC-807)
638				1								638	1	EA	SPECIAL-METER, ADJUSTED TO GRADE (COC-807)
808			1									808	1	EA	RELOCATE 8" WATER LINE (COC-STD DWG L-7401)



SCALE: 1" = 50'  
0 25 50 100

CALCULATED WRA CHECKED BJS

PROJECT SITE PLAN  
STATE ROUTE 752 AND LONG STREET INTERSECTION

PIC-752/LONG ST.

**PROJECT DESCRIPTION**  
UPGRADING 0.164 MILES OF STATE ROUTE 752 AND 0.134 MILES OF LONG STREET BY WIDENING AND RESURFACING, INCLUDING NEW STORM SEWER SYSTEM, SIDEWALKS, TRAFFIC SIGNAL, TRAFFIC CONTROL SIGNS AND PAVEMENT MARKINGS.

<b>DESIGN DESIGNATION</b>	<b>ST. RT. 752</b>
CURRENT ADT (2009).....	3600
DESIGN YEAR ADT (2030).....	6000
DESIGN HOURLY VOLUME (2030).....	510
DIRECTIONAL DISTRIBUTION.....	55%
TRUCKS (24 HOUR B&C).....	2%
DESIGN SPEED.....	35 M.P.H.
LEGAL SPEED.....	35 M.P.H.

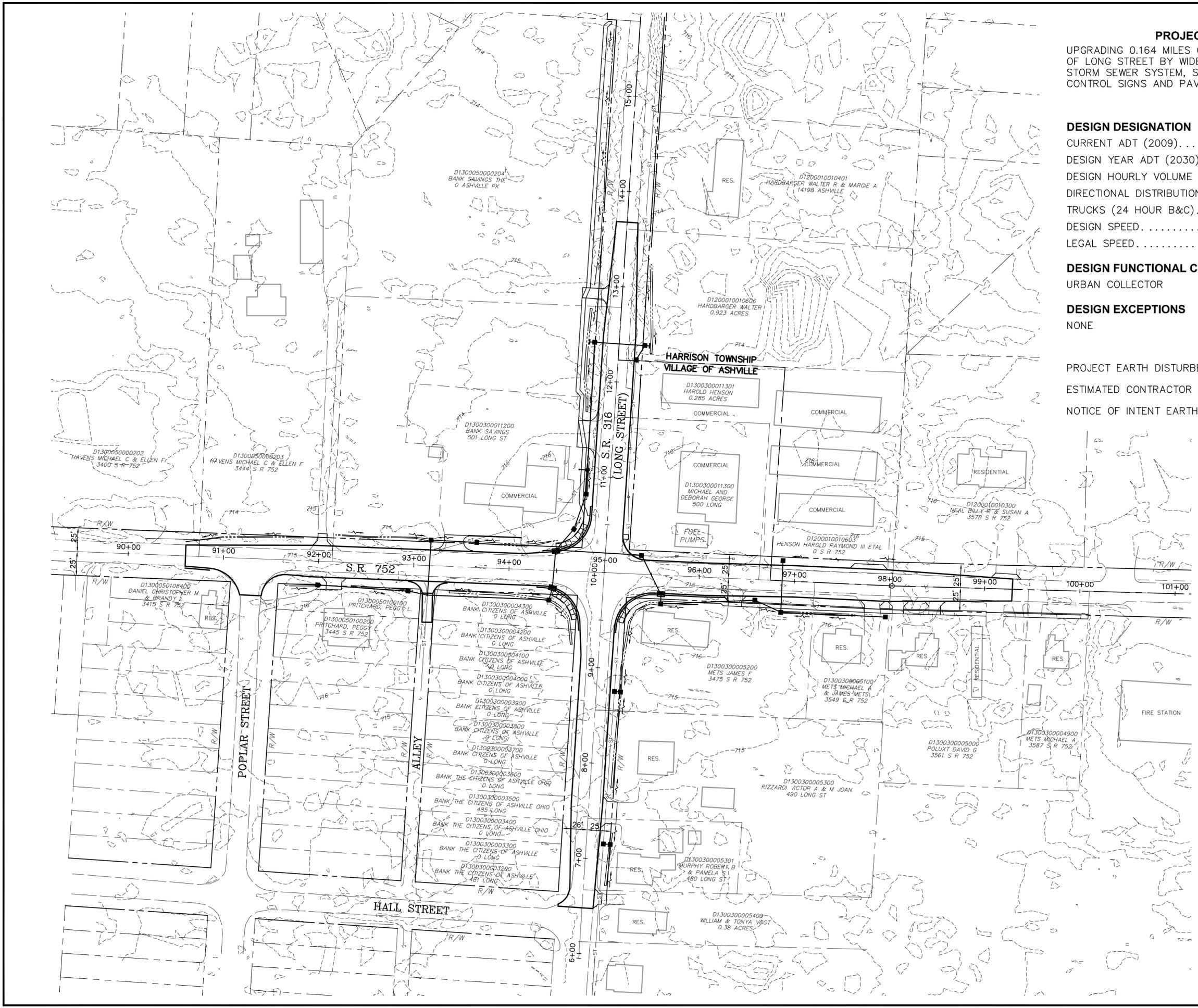
**DESIGN FUNCTIONAL CLASSIFICATION**  
URBAN COLLECTOR

**DESIGN EXCEPTIONS**  
NONE

PROJECT EARTH DISTURBED AREA:	1.7 ACRES
ESTIMATED CONTRACTOR EARTH DISTURBED AREA:	0.3 ACRES
NOTICE OF INTENT EARTH DISTURBED AREA:	4.9 ACRES

**LEGEND**

	PROPOSED STORM
	EXISTING STORM
	PROPOSED DITCH
	PROPOSED CATCH BASIN
	PROPOSED MANHOLE
	PROPOSED CURB INLET
	PROPOSED HEADWALL
	PROPOSED CLEANOUT
	PROPOSED FLOW ARROW



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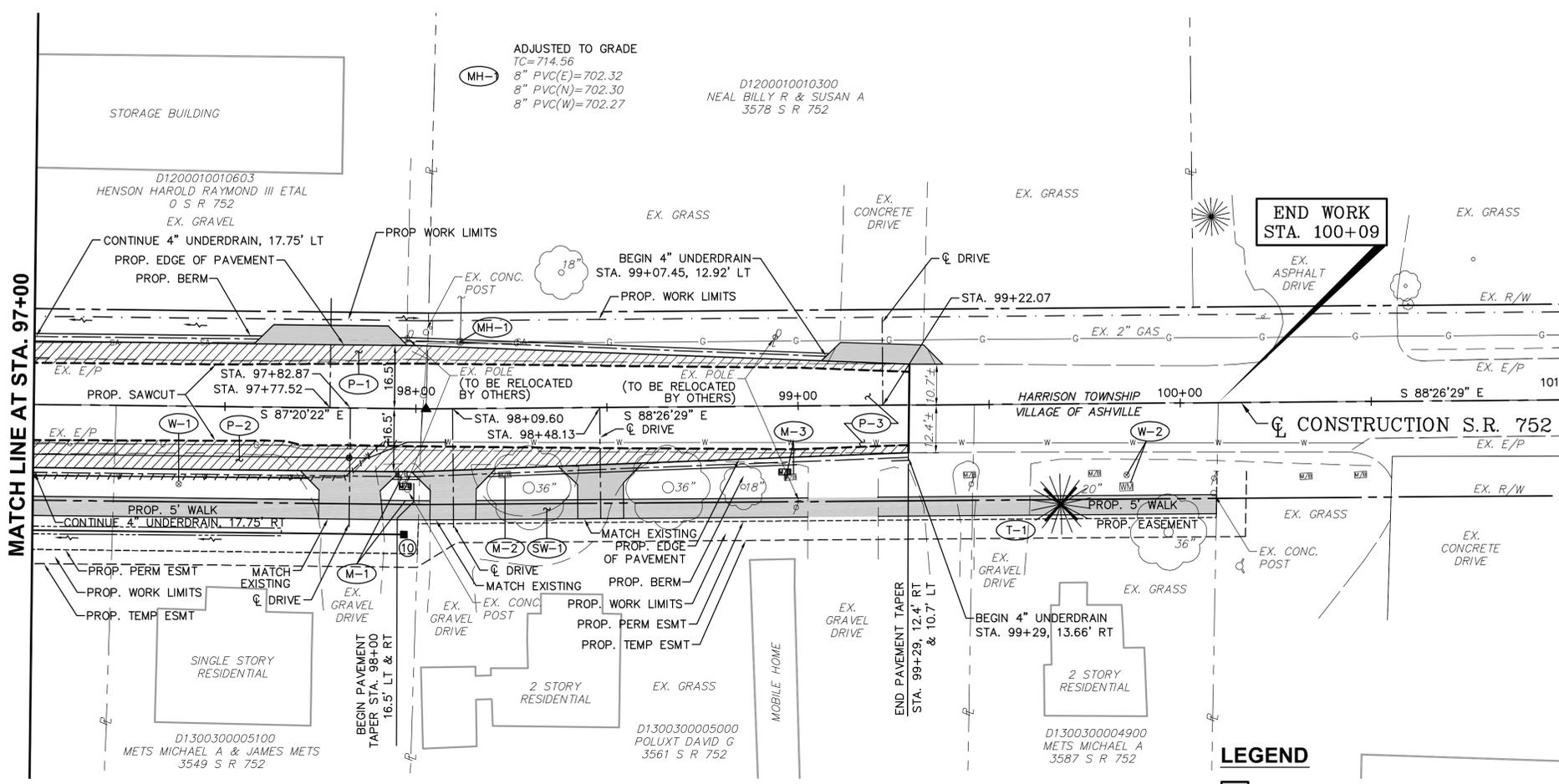




CALCULATED WRA CHECKED BJS

PLAN AND PROFILE - STATE ROUTE 752  
STATE ROUTE 752 AND LONG STREET INTERSECTION

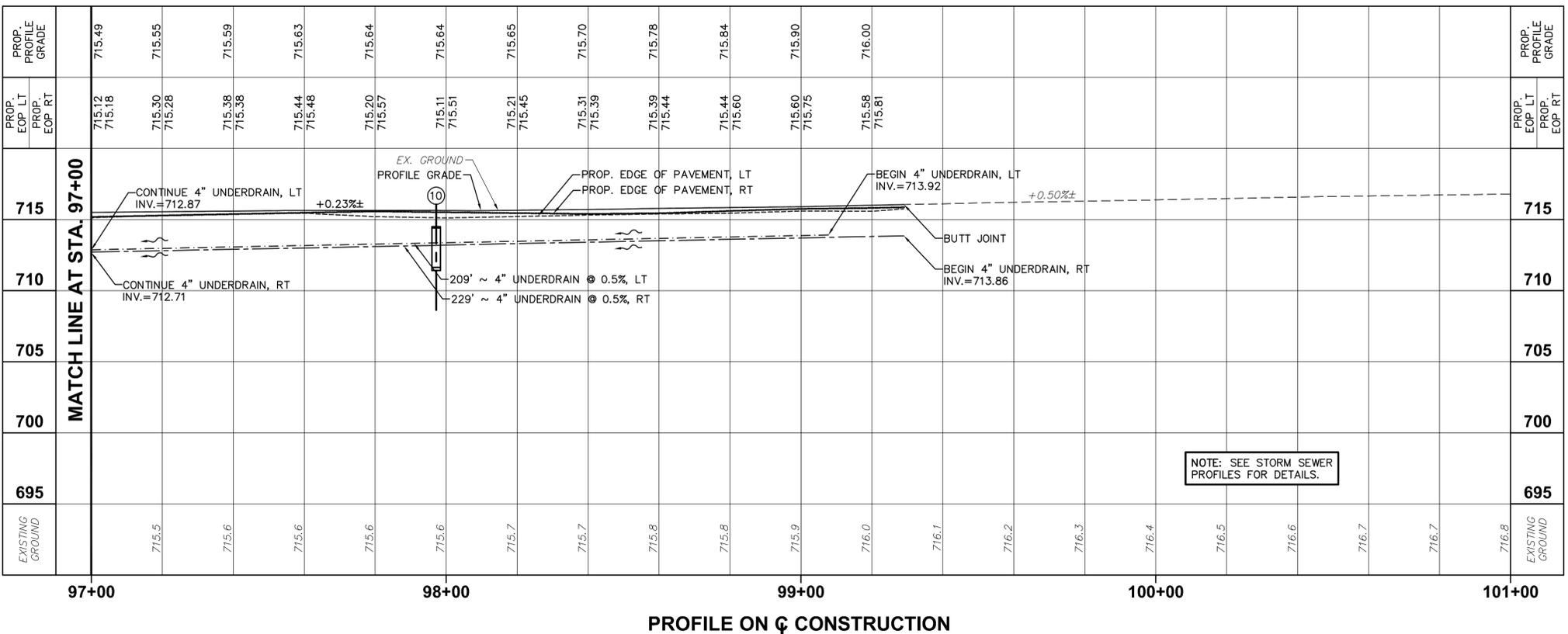
PIC-752/LONG ST.



PLAN

NOTE: FOR DRIVE QUANTITIES, SEE DRIVE DETAIL SHEET.

- LEGEND**
- = CONCRETE WALK, ISLAND AND DRIVE APPROACHES
  - = PAVEMENT WIDENING
  - = 1'06'07" LT. @ STA. 98+02.64



PROFILE ON  $\phi$  CONSTRUCTION

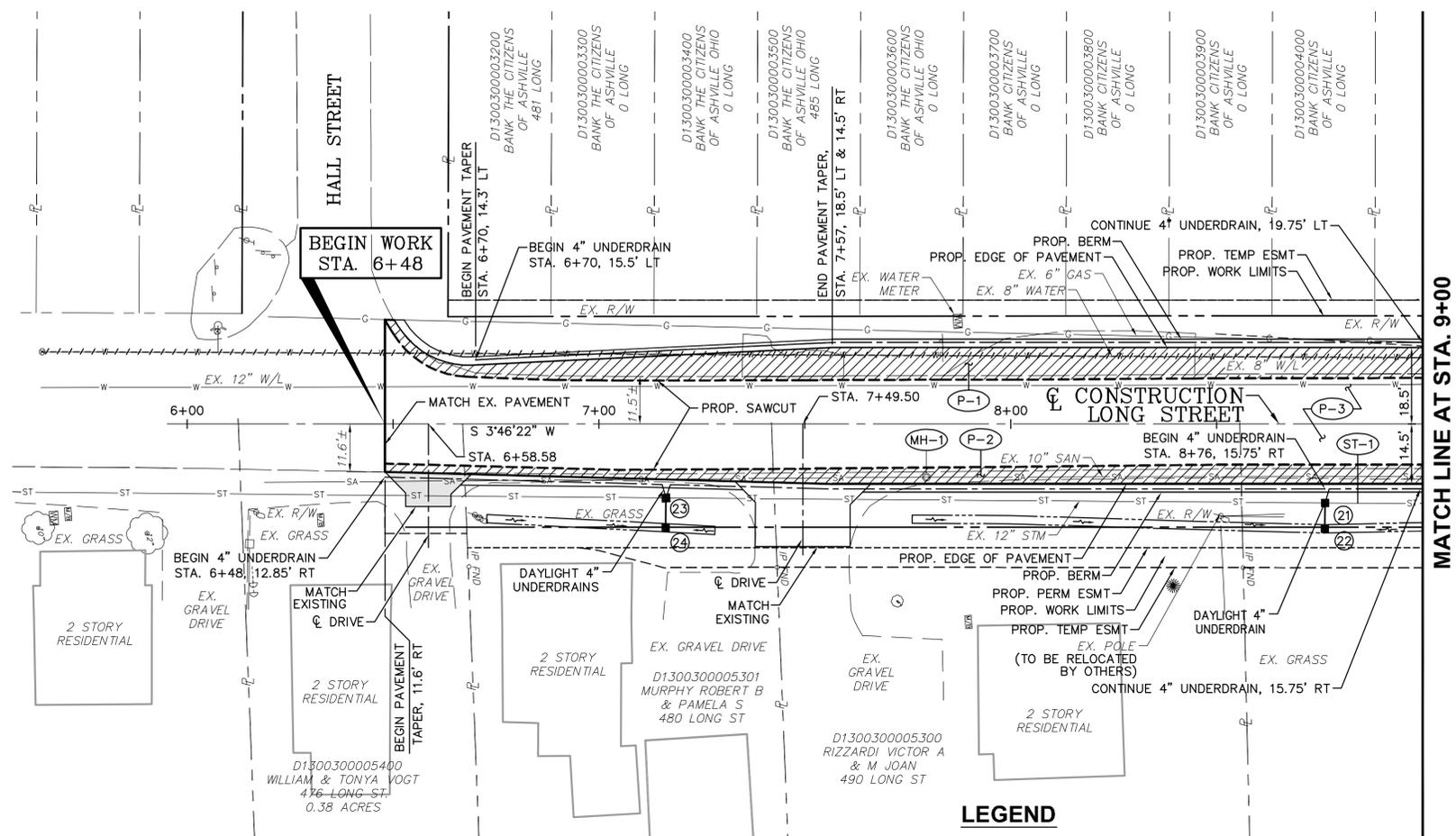
REF. NO.	STATION TO STATION	SIDE	ITEM	QUANTITY	UNIT	AMOUNT
M-1	97+97	RT	TREE REMOVED, 20" SIZE	1	EA	1
M-2	98+23	RT	PAVEMENT REMOVED	27	SY	27
M-3	98+98	RT	PAVEMENT REMOVED	27	SY	27
MH-1	98+12	LT	MANHOLE ADJUSTED TO GRADE	1	EA	1
P-1	97+00 TO 99+29	LT	4" SHALLOW PIPE UNDERDRAIN	209	FT	209
P-2	97+00 TO 99+29	RT	4" SHALLOW PIPE UNDERDRAIN	229	FT	229
P-3	97+00 TO 99+29	BOTH	4" SHALLOW PIPE UNDERDRAIN	27	FT	27
SW-1	97+00 TO 100+09	RT	CRACK SEALING, TYPE II	1414	LF	1414
T-1	99+69	RT	CRACK SEALING, TYPE II	1	EA	1
W-1	97+38	RT	CONCRETE WALK	1	SF	1
W-2	99+86	RT	CONCRETE WALK	1	SF	1
TOTAL TO GENERAL SUMMARY				1		1



CALCULATED WRA  
CHECKED BJS

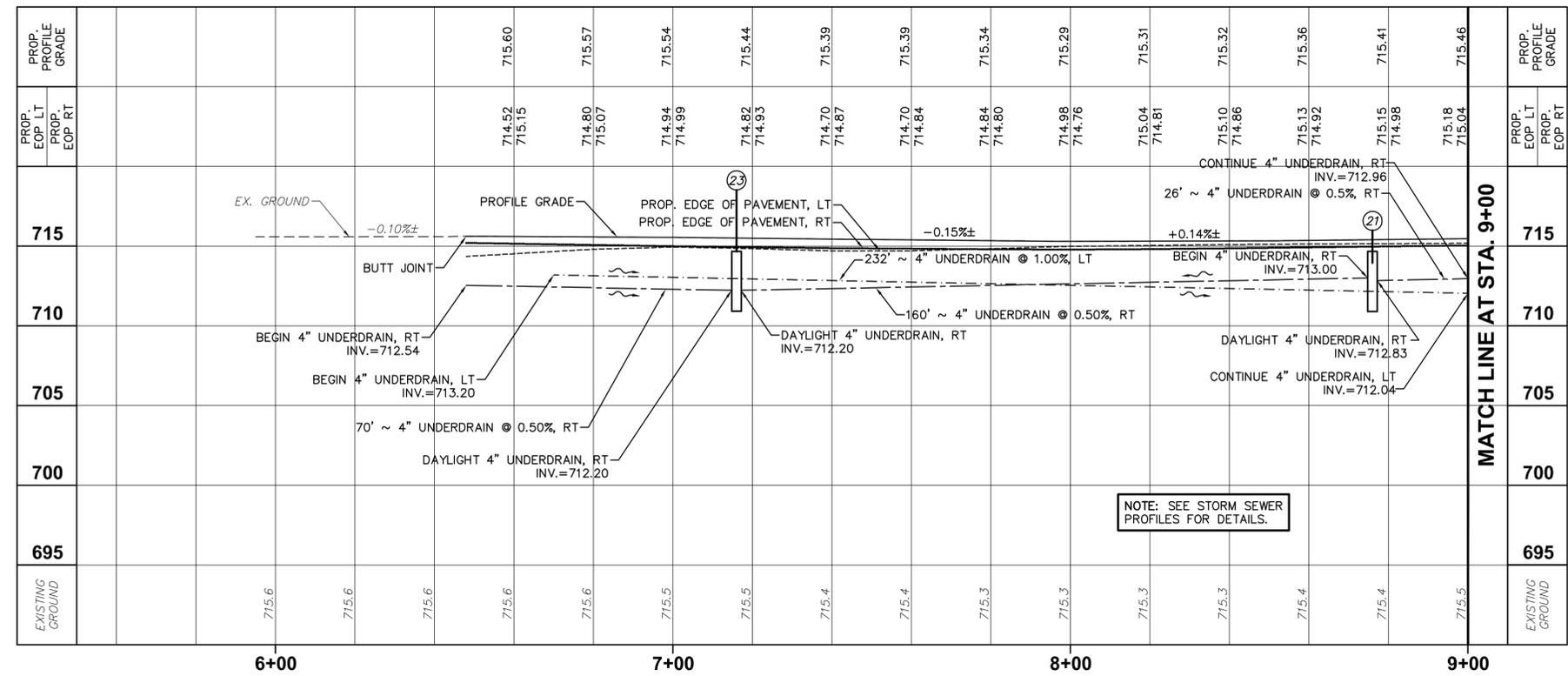
# PLAN AND PROFILE - LONG STREET STATE ROUTE 752 AND LONG STREET INTERSECTION

## PIC-752/LONG ST.



### LEGEND

- [Solid Grey Box] = CONCRETE WALK, ISLAND AND DRIVE APPROACHES
- [Hatched Box] = PAVEMENT WIDENING



REF. NO.	STATION TO STATION	SIDE	ITEM	QUANTITY	UNIT	AMOUNT
MH-1	7+79	RT				
P-1	6+70 TO 9+00	LT				
P-2	6+48 TO 9+00	RT				
P-3	6+48 TO 9+00	BOTH				
ST-1	8+76 TO 9+00	RT				
			TOTAL TO GENERAL SUMMARY	24	64	428
				24	64	428
				32	32	202
				32	202	454
				33	23	308
				11	9	61
				20	66	135
				61	41	762
				61	41	762
				79	56	454
				23	18	308
				31	31	254
				9	5	232
				1	1	486
				1	1	486
				14	31	762
				14	31	762
				9	5	232
				5	31	454
				232	254	486

(MH-1)  
ADJUST TO GRADE  
TC=714.85  
10" PVC(N)=701.01  
10" PVC(S)=700.94

(21)  
ADJUST TO GRADE  
TC=714.67  
12" CPP(N)=710.78  
(TO BE ABANDONED)  
12" RCP(S)=710.64

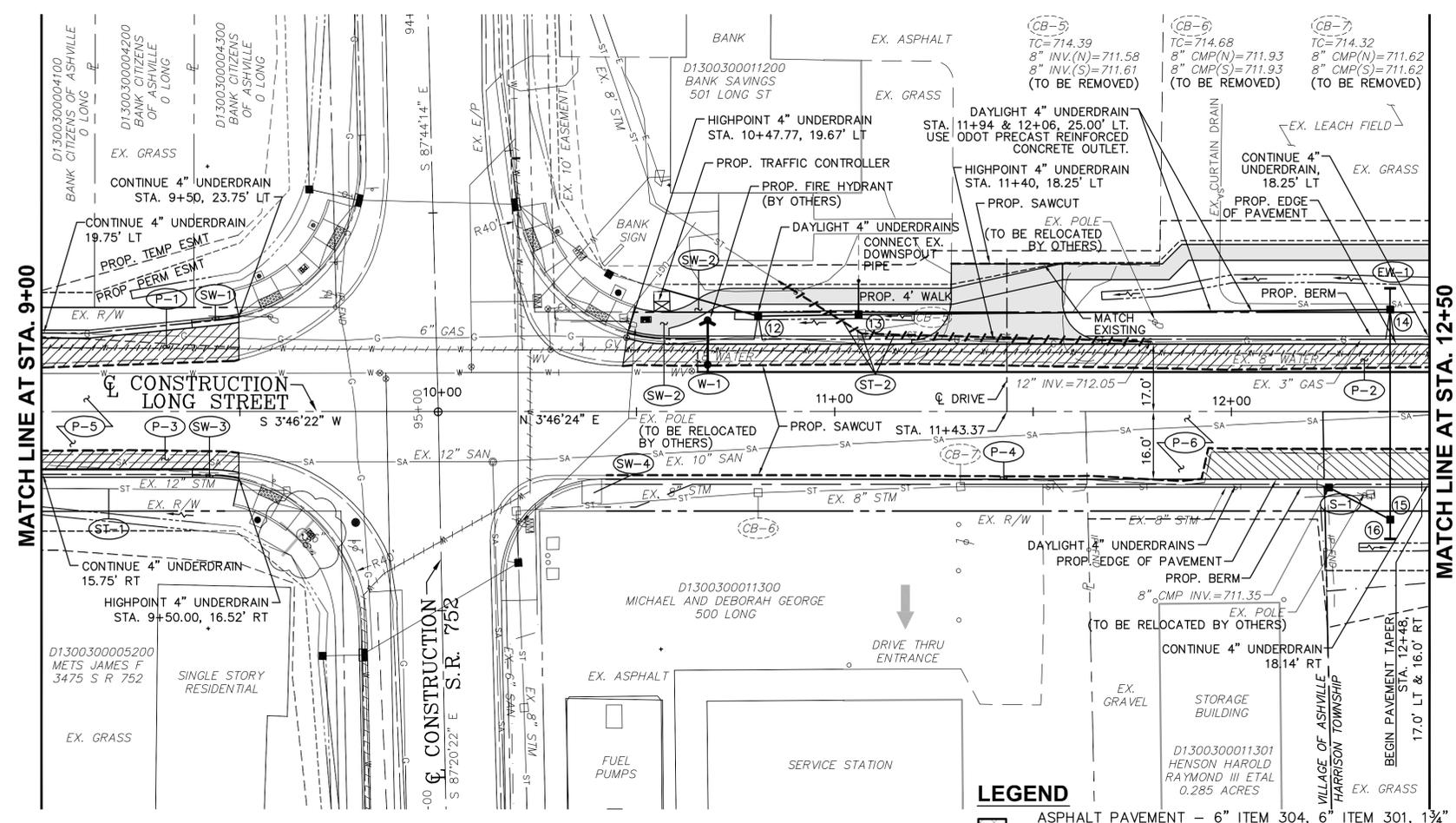
(23)  
ADJUST TO GRADE  
TC=714.67  
12" RCP(N)=710.81  
12" RCP(S)=710.90



CALCULATED WRA CHECKED BJS

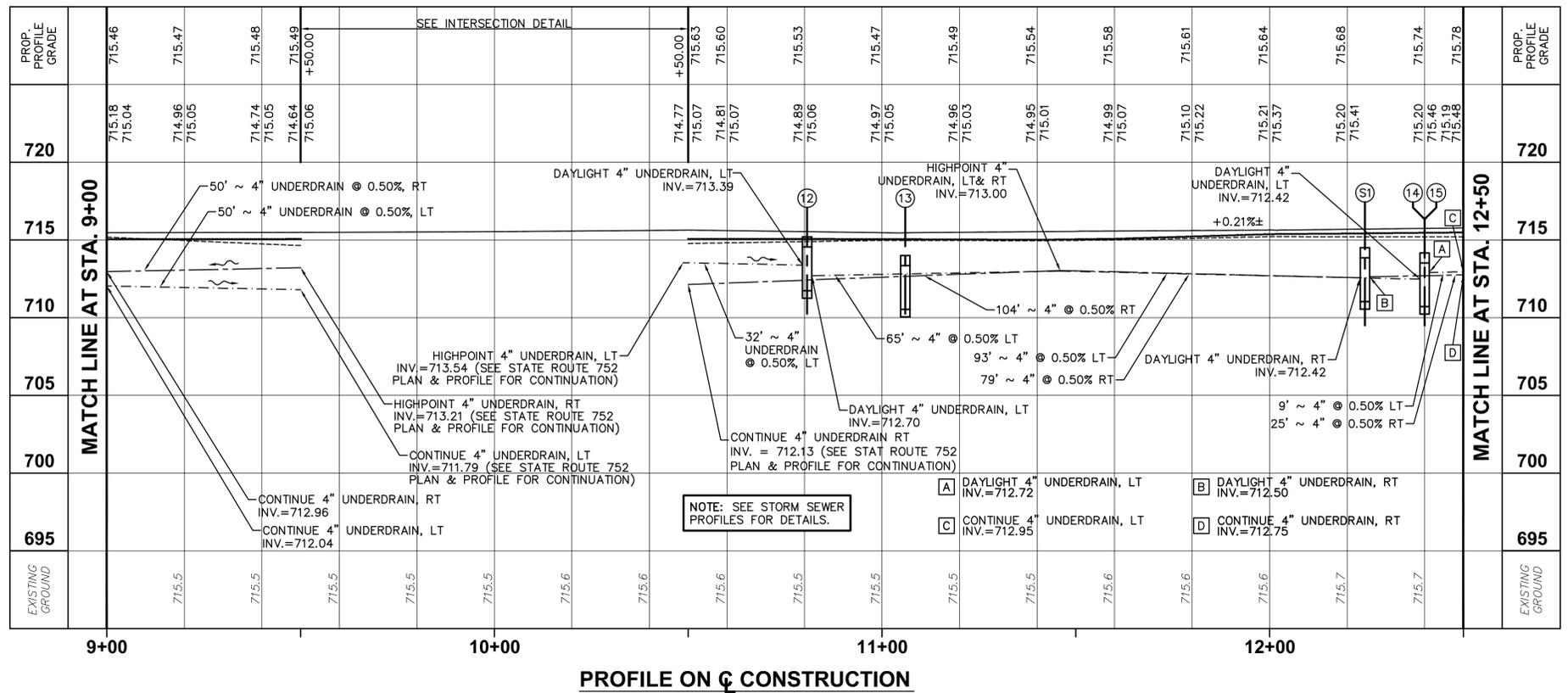
# PLAN AND PROFILE - LONG STREET STATE ROUTE 752 AND LONG STREET INTERSECTION

## PIC-752/LONG ST.



NOTE: FOR DRIVE QUANTITIES, SEE DRIVE DETAIL SHEET.

- LEGEND**
- [Hatched Box] = ASPHALT PAVEMENT - 6" ITEM 304, 6" ITEM 301, 1 1/4" ITEM 448 INTERMEDIATE COURSE AND 1 1/4" ITEM 448 SURFACE COURSE
  - [Diagonal Lines] = PAVEMENT WIDENING
  - [Solid Grey Box] = CONCRETE WALK, ISLAND AND DRIVE APPROACHES



REF. NO.	STATION TO STATION	SIDE	ITEM	QUANTITY	UNIT
CM-1	10+41	RT	SPECIAL-VALVE BOX, ADJUSTED TO GRADE (COC-807)	1	EA
P-1	9+00 TO 9+49	LT	CONCRETE CURB (COC STD DWG 2010)	11	FT
P-2	10+47 TO 12+50	LT	CONCRETE WALK	704	SF
P-3	9+00 TO 9+47	RT	4" SHALLOW PIPE UNDERDRAIN	23	FT
P-4	10+41 TO 12+50	RT	CONCRETE WALK	21	SF
P-5	9+00 TO 9+49	BOTH	4" SHALLOW PIPE UNDERDRAIN	12	FT
P-6	10+41 TO 12+50	BOTH	CONCRETE WALK	23	SF
ST-1	9+00 TO 9+50	RT	PRECAST REINFORCED CONCRETE OUTLET	2	EA
ST-2	10+84 TO 11+79	LT	1 1/4" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE II	2	CY
ST-3	10+42 TO 12+25	RT	1 1/4" ASPHALT CONCRETE SURFACE COURSE, TYPE I	7	CY
SW-1	9+40 TO 9+50	LT	CRACK SEALING, TYPE II	49	FT
SW-2	10+47 TO 12+50	LT	BITUMINOUS PRIME COAT (BERM)	13	GAL
SW-3	9+40 TO 9+50	RT	BITUMINOUS PRIME COAT	84	GAL
SW-4	10+41 TO 10+46	RT	TACK COAT	3	GAL
W-1	10+64	LT	9" AGGREGATE BASE (BERM)	2	CY
EW-1,16	12+40	LT	6" AGGREGATE BASE	37	CY
TOTAL TO GENERAL SUMMARY				60	